

# PRINCE FREDERICK MASTER PLAN & ZONING ORDINANCE



Prepared by the Calvert County Department  
of Planning & Zoning

Adopted July 11, 1989

Amended:

2/15/94	4/2/96	4/20/99	12/2/03
6/13/95	12/23/97	5/29/01	5/18/04
3/12/96	1/20/98	7/3/01	8/10/04
3/19/96	12/8/98	10/28/03	5/1/06

**PLANNING COMMISSION INTERPRETATION  
SECTION IV.D  
PRINCE FREDERICK ZONING ORDINANCE**

The following interpretation was adopted by the Planning Commission on December 10, 1997:

Section IV.D of the Prince Frederick Zoning Ordinance which states that “All mechanical equipment shall be screened from view”, shall be interpreted to mean that mechanical equipment shall not be visible from public roads or adjacent properties and that the design of utility screens shall be subject to Architectural Review and approval in accordance with established procedure.

**PRINCE FREDERICK  
MASTER PLAN**

RESOLUTION NO. 27-89

(A Resolution Adopting the Prince Frederick Master Plan)

WHEREAS, pursuant to the authority contained in Article 66B of the Annotated Code of Maryland, the County Commissioners of Calvert County, Maryland have general powers to promulgate zoning ordinances and master plans; and

WHEREAS, on September 2, 1988, the Planning Commission held a public meeting and considered the proposed Ordinance and Master Plan for Prince Frederick; and

WHEREAS, on November 21, 1988, the Planning Commission approved the Plan and Zoning Ordinance and recommended adoption of the same to the Board of County Commissioners; and

WHEREAS, the Board of County Commissioners held a public meeting on December 8, 1988 and considered the proposed Ordinance and Master Plan; and

WHEREAS, on February 28, 1989, the Board of County Commissioners unanimously adopted both the Plan and Ordinance without substantive changes.

NOW, THEREFORE, BE IT RESOLVED, by the Board of County Commissioners of Calvert County, that the adoption of the Prince Frederick Master Plan and Ordinance, which is attached hereto and made a part hereof, Be and hereby Is approved as the official Master Plan and Zoning Ordinance for the Prince Frederick Town Center.

DONE, this 11<sup>th</sup> day of July, 1989 by the Board of County Commissioners of Calvert County, Maryland, sitting in regular session.

ATTEST:

Mary J. Watson  
Mary Watson, Clerk

BOARD OF COUNTY COMMISSIONERS  
OF CALVERT COUNTY, MARYLAND

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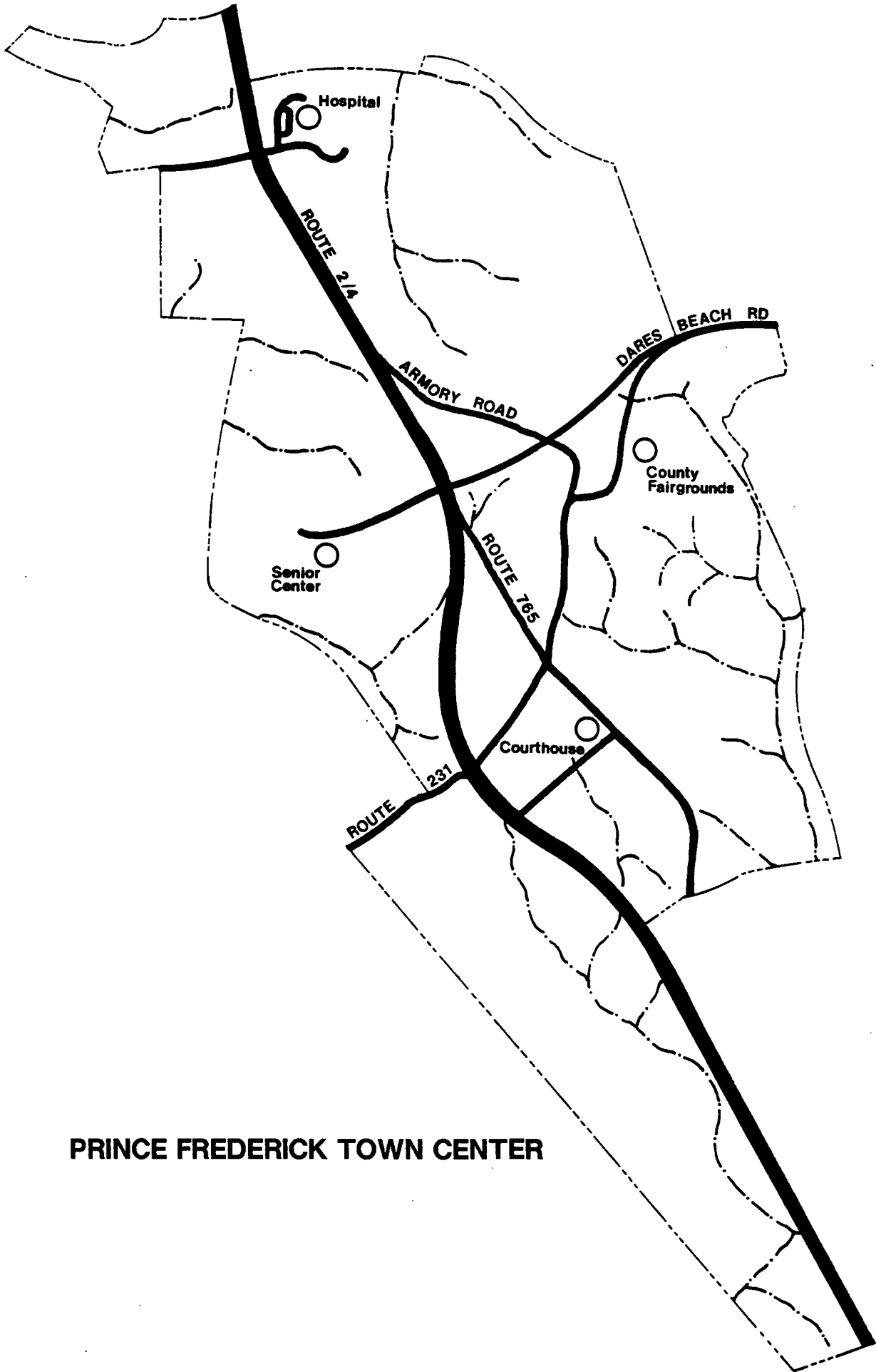
Barbara Stinnett  
Barbara Stinnett

**PRINCE FREDERICK MASTER PLAN  
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The Prince Frederick Master Plan was prepared by the Calvert County Department of Planning and Zoning with assistance from the following consultants:

ARI Engineering - Traffic Study  
McCrones Inc. - Streetscape Study



**PRINCE FREDERICK TOWN CENTER**

## I. INTRODUCTION

**KEY ISSUE: How can Prince Frederick be not only pleasant and prosperous but also beautiful - a source of pride to County residents?**

### A. INTRODUCTION

Successful communities are the result of forethought and careful planning. The quality of life in a town is too important to be left to chance. To be effective, a plan must take into consideration aesthetics as well as economic concerns. It must give the community a vision of what it could be.

### B. OBJECTIVES OF THE COMPREHENSIVE PLAN

The 1983 Comprehensive Plan calls for a Master Plan to be developed for each Town Center establishing land use policies based on desirable economic development, the prevention of hazardous traffic situations, public facilities available and needed, residential development, aesthetics, compatibility with and protection of existing and previously planned uses and the cumulative effect upon the County. The purpose of having a Master Plan is to address these issues in a detailed, comprehensive manner.

### C. FUNCTION OF A MASTER PLAN

A Master Plan is a joint agreement between the public and the private sectors. Private individuals agree to be guided by the restrictions and requirements in the plan and in return, the government agrees to invest in the necessary public improvements such as roads, sidewalks, street lights and water and sewer lines.

### D. IMPLEMENTATION

The implementation of this plan will be primarily accomplished through the Prince Frederick Zoning Ordinance and the Capital Improvements Program.

The Zoning Ordinance is a legal document which regulates land use within the Town Center.

The Capital Improvement Program lists capital improvement projects that are scheduled to be funded by the County. Projects that are to be included in the Capital Improvement Program are listed in Chapter IX.

Other planning documents are also used to implement the plan. These include the Water and Sewerage Plan, the Solid Waste Plan and the Land Preservation and Open Space Plan. However, many aspects of the plan fall outside the realm of regulation. Their implementation depends upon whether or not citizens take an active interest in promoting them.

### E. THE "DO NOTHING" ALTERNATIVE

During the next several decades, Prince Frederick will face major changes. The population is expected to grow rapidly. Traffic will increase substantially as new stores and services locate in Prince Frederick. What will happen if there is no master plan for the Town Center?

. There will be at least eight traffic signals. Cars will have to wait for the light to change more than once. Even with signals, it will be extremely difficult to get out onto Route 4 from either end of Main Street, and from Duke Street.

. Virtually all commercial development will be located along Rte 2/4 and space for additional commercial development will be limited due to a lack of adequate road frontage.

. People driving on Rte 2/4 will be reluctant to leave Rte 2/4 because of the difficulty of getting back onto the highway.

. There will be limited protection for residential communities and unique cultural and natural features.

## F. COMMUNITY GOALS AND OBJECTIVES

On April 1, 1987, a public workshop was held at the Calvert Middle School to identify the problems and concerns which the residents of Prince Frederick felt should be addressed by this Master Plan. Most of the issues identified by the citizens can be divided into two categories - "quality of life" issues and economic development issues.

Quality of life issues - Many of the recommendations received have been phrased using words such as "preserve", "protect" and "maintain", indicating that growth is perceived by many to represent a threat to the existing quality of life in Prince Frederick.

Economic development issues - Many of the recommendations received have been phrased using words such as "promote", "encourage" and "pursue", indicating that growth is perceived to represent opportunities for significant improvements in the economy and should, therefore, be actively advanced.

The issues are listed below in order of priority:

1. Ensure that public facilities (water, sewer and roads) are adequate to support growth.
2. Maintain high standards of road safety and minimize traffic congestion.
3. Preserve the historic heritage of Prince Frederick.
4. Promote pleasant, stable residential communities.
5. Preserve existing natural features (forests, large trees, stream valleys and natural areas).
6. Promote economic development.

## G. COUNTY ACTIONS

The County has taken the following actions prior to the enactment of this Master Plan which are designed to address several of the concerns raised by citizens during the planning process:

1. directed staff to conduct a study of the water system for Prince Frederick that will ensure adequate water flow and pressure to meet fire protection requirements.
2. authorized the construction of a land treatment wastewater facility with an initial capacity of 800 edu's.
3. enacted an Adequate Facilities Ordinance to help ensure that schools and roads are adequate to support growth.
4. contracted a study of government space needs (CADRE study).

## H. HOW TO READ THIS PLAN

This Plan is not meant to be read from cover to cover. It is broken down into nine chapters dealing with different aspects of the Town Center.

In most of the chapters, the key issues are presented at the very beginning followed by the relevant objectives and recommendations from the Comprehensive Plan. The next section usually describes the background and the existing situation. If appropriate, options are identified with advantages and disadvantages for each option. The final section gives the actions which are actually recommended by the Plan. The last chapter discusses the implementation and financing of the recommendations contained in this Plan.

## II. EXISTING SITUATION

Prince Frederick Town Center - 1988:

Area - 1,735 acres

Population - Approximately 700

Distance from D.C. - 46 miles

Distance from Baltimore - 64 miles

### A. DESCRIPTION, GEOGRAPHIC LOCATION AND SIZE

Prince Frederick is the capital of Calvert County and the largest of the Town Centers. It is the major employment center for both the public and private sector. It contains the largest concentration of public and quasi-public services.

Prince Frederick is located 46 miles from Washington, D.C. and 64 miles from Baltimore. It contains 1,735 acres of land which were zoned Town Center District during the Comprehensive Rezoning adopted by the County in 1984. The population of Prince Frederick is approximately 700 persons.

### B. NATURAL FEATURES

Prince Frederick is surrounded and heavily dissected by stream valleys and steep slopes. These features have largely determined the development pattern in Prince Frederick which has both a strong north/south orientation (following the major drainage divide) and a tendency to form small isolated clusters in areas of relatively flat topography. The major stream valleys drain into Hunting Creek and Parker's Creek. Forests cover a considerable amount of land within the Town Center.



## C. HISTORY

The land which comprises the present day Prince Frederick was originally part of a tract known as "Williams' Old Fields". When a decision was made to relocate the County seat there, it was renamed Prince Frederick in honor of the eldest son of King George I of England. Funds were appropriated by the Assembly in 1722 and in 1725 to build a courthouse and a jail at the new site.

In 1882, Prince Frederick was completely destroyed by fire. The new courthouse, built in 1916, has been called "one of the most beautiful courthouses in Maryland".

In an article written for the Calvert Historian, Ailene Hutchins presents research indicating that a grove of trees stood directly across from the Old Calvert Hotel in the early part of this century. The Wisteria at the end of Duke Street then stood in the front yard of the Calvert Hotel surrounded by a white picket fence. A Town Hall stood between the grove and the Courthouse. Many of the houses along Main Street were built during the late 19th and early 20th century.

In the late 1950's, the new Route 2/4 was constructed bypassing downtown Prince Frederick. The Prince Frederick Shopping Center was built at the corner of Route 231 and Route 2/4. Many businesses moved in order to be visible from the new highway, forming a cluster of commercial development around the new shopping center. In the early 1970's, the Calvert Village Shopping Center was built at the Dares Beach Road intersection.

In the early 1980's, Goldstein's department store across from the Courthouse on Main Street was converted into an office building for county workers. Many of the old houses along Main Street were converted to lawyers' offices. In 1984, Dr. Briscoe's office and home was restored and opened as the Old Field Inn.

## D. EXISTING DEVELOPMENT

### 1. Commercial Development

Most consumer businesses are located along Route 2/4. They are largely limited to the category of convenience goods and services much as they were in the past. This category features items customers purchase frequently. Over the last several years, efforts have been made repeatedly to break into the category of comparison goods and services such as shoe stores and clothing stores. However, very few of these types of stores are located in Prince Frederick. Specialty stores have also made several attempts at gaining a foothold in Prince Frederick but, so far, they have had little success.

### 2. Offices

Professional offices tend to cluster near the Courthouse. A number of offices are also located in the vicinity of the Calvert Village Shopping Center. Medical offices tend to cluster near the hospital.

### 3. Residential Development

There are fewer than 300 residences in Prince Frederick clustered in two primary locations:

- a. Along Armory Road and Fairgrounds Road, and
- b. Along Route 765, south of the Courthouse.

Most single-family detached residences are located on lots ranging from 1/2 to 1 1/2 acres in size. The highest density for single-family attached residences is 5.3 units per acres. The largest apartment complex is Calvert Pines on West Dares Beach Road, off the west side of Route 2/4, with 50 units. In both of the primary residential areas, there is a mix of single-family detached and single-family attached residences.

The existing pattern of residential development provides a comfortable mix of housing types within its neighborhoods. They have the flavor and appearance of a typical neighborhood of single family detached homes, but they provide a small number of higher density housing units along their edges. This pattern provides the kind of flexibility that is needed in order to allow residents to meet changing household circumstances without necessarily having or wanting to move to new neighborhoods.

## E. PUBLIC FACILITIES AND SERVICES

### 1. Roads

The road system in Prince Frederick is currently operating at excellent to good levels of service according to national traffic standards. However, there are existing problems at a number of road intersections and crossovers. In addition, the rate of increase in traffic volume indicates that the road system will fail at certain locations prior to 1995. A detailed road analysis is presented in Chapter VIII.

### 2. Schools

There are four schools along Dares Beach Road to the east of the Town Center. The Calvert High School site is also the site of the Planetarium and the high school auditorium. Currently, schools in the vicinity are near or at capacity. Public schools currently serving the Prince Frederick Town Center are: Calvert Elementary, Calvert Middle, and Calvert High.

### 3. Wastewater Treatment

The existing Prince Frederick sewage treatment plant is at capacity. Plans are underway for a land treatment facility which is scheduled to be completed by 1992. Based on an initial capacity of 800 edu's, this facility will be able to serve up to 2,000 households and three times the existing amount of commercial, office, public and industrial uses. Uses which may generate toxic wastes will be required to provide on-site pre-treatment of wastewater before it reaches the public treatment facility.

### 4. Water System

Groundwater is adequate to supply water needs beyond the year 2010. A water conservation program is being carried out in Calvert County and a County funded retrofit program is in effect in Prince Frederick. The 1986 Calvert County Comprehensive Water and Sewer Plan's first priority is the extension of service within the Prince Frederick Sanitary District and the increase of flow and pressure to meet fire protection requirements. The water system for fire protection is not adequate at this time. The Calvert County Water and Sewer Plan is updated biennially. Copies are available at the Department of Planning & Zoning, Room 1, Courthouse Annex.

### 5. Fire, Rescue and Law Enforcement Services

Fire, rescue and law enforcement facilities are periodically evaluated in the Calvert County Fire, Rescue and EMS Master Plan. The plan is scheduled to be updated in FY 1990.

### 6. Community Services

The main branch of the Calvert County Library, Calvert Pines Senior Citizens Center, the Calvert County Health Department, the Calvert County Association for Retarded Citizens and Bless, Inc. are among the major community services available in Prince Frederick.

### 7. Recreational Facilities

Residents in the 2nd Election District are currently served by Hallowing Point Park, which provides playing fields, tennis courts, an outdoor stage, and a fitness trail. The park is at capacity in terms of its ability to provide space for organized team sports. The park is located west of the Prince Frederick Town Center on Route 231.

### 8. Courts and General Government Services

The Courthouse in Prince Frederick continues to serve as the center of County government, housing the District Court, the Circuit Court and the Orphans Court in addition to the offices of the Board of County Commissioners. All of the County Departments are located either in the Courthouse or in buildings immediately adjacent to the Courthouse. A space needs study completed in November, 1988 (CADRE study), indicates the immediate need for a substantial increase in office space.



### III. A VISION FOR PRINCE FREDERICK

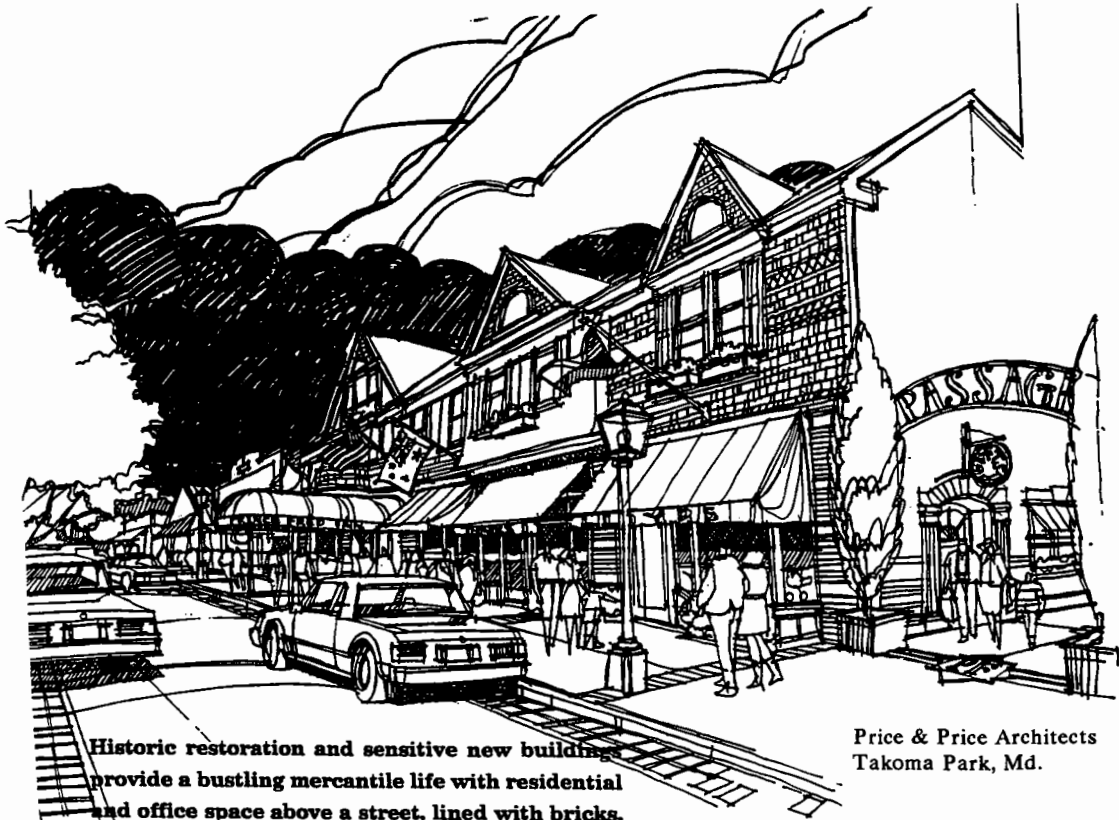
One of the major purposes of a Master Plan is to provide an overall concept or vision of what citizens want their community to be. Such a vision should serve as a guide to decision-makers and help ensure that new development will contribute toward establishing a positive identity for the community. Below is a vision of what Prince Frederick can be.

Old Prince Frederick will be much busier than it is today, more like it was in the old days, before the by-pass was built. But it will still retain its late 19th and early 20th century architectural character and its old town pattern of closely spaced buildings and narrow roads. The Courthouse will continue to be the hub of old Prince Frederick, the scene of comings and goings of office workers, attorneys and citizens.

Spreading out from the Courthouse, along Main Street, Duke Street, and Church Street, homes, offices, public buildings, churches, restaurants and stores will gradually blend into quiet residential areas. The lake beyond the new State Office Building, with its winding path, park benches and outdoor lighting, will make an ideal setting for offices and homes.

In the evening and on weekends, county residents will be drawn to the area by the growing number of shops and restaurants and the activities taking place on the Courthouse green such as craft shows and concerts.

Most of the new residential development along Church Street and lower Armory Road will be town houses to serve people who like Prince Frederick's old town atmosphere and the convenience of walking to stores and services. Farther out on Armory Road and south along Main Street, residential neighborhoods will continue in the tradition of a small town.



Historic restoration and sensitive new buildings provide a bustling mercantile life with residential and office space above a street, lined with bricks, awnings, benches, and pedestrian scaled lights.

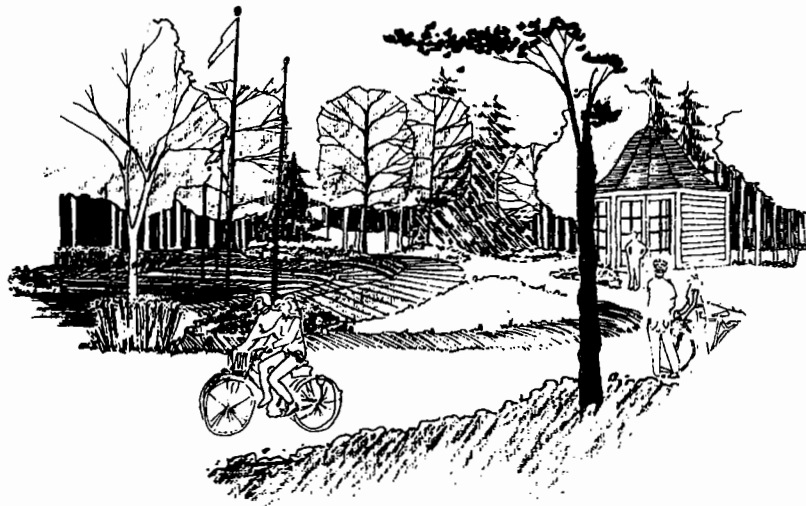
Price & Price Architects  
Takoma Park, Md.

Continuing the vision of what Prince Frederick can be...

On the east side of Route 2/4, Armory Road will extend north to provide access to the hospital and a new planned community via a parkway. The road will skirt a new lake on the east side before tying back into Route 4 north of Prince Frederick. The lake will be a major focal point for the Town Center. On one side will be stores and the hospital and on the other the new planned community will provide a variety of attractive housing types taking advantage of the scenic views of the lake. A trail will circle the lake providing pedestrian and bicycle access between the residential and commercial areas. Willow trees, flowering shrubs and lamp posts will create a parklike atmosphere.

On the west side of Route 2/4 between Stoakley Road and Route 231, a new service road will allow space for future stores and services as well as higher density residential development. As it develops, it will become a new "Main Street" permitting Rte 2/4 to perform its primary function as a through road.

Even with increased development, much of Prince Frederick will retain its trees and groundcover due to its stream valleys and sensitive steep slopes. As landscaping along roadways matures, the town will become increasingly more attractive. Sidewalks and trails throughout the Town Center will allow people to enjoy its attractive features.



#### IV. RESIDENTIAL DEVELOPMENT

##### KEY ISSUES:

**What kind of housing is needed to meet the needs of the population by the year 2010?**

**What is the maximum density that should be permitted in the Town Center?**

**How can the stress of rapid population growth be minimized?**

##### A. PURPOSE

Prince Frederick can expect a substantial increase in population within the next 10 to 15 years and beyond. Rapid growth in any community is a source of deep concern to existing residents. It is of particular concern in Prince Frederick because the ratio of undeveloped land to developed land is so high. One purpose of the Prince Frederick Master Plan is to promote the continuation and creation of pleasant, stable neighborhoods serving persons with a wide range of interests, incomes and ages.

##### B. OBJECTIVES OF THE COMPREHENSIVE PLAN

The first objective of the housing section of the Comprehensive Plan is to: "Encourage the availability of a variety of housing types which can meet the needs of different age groups, family sizes, life styles and income capabilities for Calvert County residents. Place a special emphasis on providing housing opportunities for low and moderate income families of the County through public and private actions."

Calvert County has an obligation to provide a variety of housing types. Much of the County consists of large lots with single-family houses. Three areas were chosen to locate higher density housing: Prince Frederick, Solomons, North Beach and Chesapeake Beach. They were chosen because of the availability of water and sewer, shopping and public facilities.

##### C. POPULATION PROJECTIONS

The current population in Calvert County is approximately 50,000 (Jan., 1989). Assuming trend growth (approximately 1000 new dwelling units per year at 2.4 persons per household), the population will reach 75,000 by the year 2000 and 100,000 by the year 2010.

Population projections based on past trends do not take into account the expanded capacity for growth generated by existing or planned sewerage treatment facilities in each of the three major Town Centers. (Solomons, Prince Frederick and North Beach/Chesapeake Beach).

Each existing or planned facility will be able to accommodate up to 5,000 residents in Phase I and up to 10,000 residents at full capacity. Taken as a whole, current or planned sewerage treatment facilities can serve approximately 30,000 residents. It is anticipated that the major Town Centers will absorb some of the projected 1000 new dwelling units per year and that they will contribute to an escalation in the overall rate of growth by opening up new housing markets that are currently not being served. These markets include County residents who are unable to find affordable housing in the County as well as additional residents drawn from outside the County.

While the capacity of the planned sewerage treatment plant provides an upper limit to the population that can be served by existing or planned sewerage treatment facilities, (approximately 4,000 households or 10,000 residents) there are no data upon which to base a projection for the rate of population growth within the Town Center.

**D. DENSITY OPTIONS**

The County-wide Zoning Ordinance permits a maximum density of 14 units per acre in Town Centers. The reason why this density was adopted is that a lower density does not provide for the development of apartments and townhouses. Suburban counties permit garden apartments at a density of 16-18 units per acre and mid-rise apartments at a density of 20-40 units per acre. The standard for townhouse and low density apartments has generally been 14 units per acre, the same as the County-wide Zoning Ordinance. The two existing residential neighborhoods in Prince Frederick are developing at a density of less than three units to the acre.

<b>OPTION #1 Continue to permit 14 dwelling units per acre.</b>	
Advantages:	Disadvantages:
1. Provides the greatest opportunity for higher density housing.	1. Will alter the existing character of residential development in Prince Frederick.
2. Allows the market to determine where the high density will go.	2. All properties would tend to develop at the highest density until the available capacity is used.
	3. Attracts people from outside the County seeking affordable housing.
<b>OPTION #2 Reduce the density in the entire Town Center.</b>	
Advantages:	Disadvantages:
1. Reduces the impact on existing single family communities.	1. Reduces the opportunity for affordable housing.
2. Makes it easier to program public facilities.	2. Lessens the financial incentive to build alternative housing types.
<b>OPTION #3 Reduce the density in specific areas.</b>	
Advantages:	Disadvantages:
1. Reduces the impact on existing single-family communities.	1. Limits opportunity for affordable housing.
2. Reduces the potential impact on public facilities and services.	2. Will exclude certain housing types in some areas.
	3. Boundaries may be hard to justify in areas that are currently undeveloped, causing rezoning requests.
<b>OPTION #4 Promote mixed-density communities which provide a variety of housing types within individual neighborhoods.</b>	
Advantages:	Disadvantages:
1. Would be consistent with the existing pattern of development in the Town Center.	1. Would require more careful site design in order to ensure compatibility between different housing types.
2. Would avoid large concentrations of high density in any given area.	2. Could prove infeasible for smaller tracts of land.
3. Would provide maximum flexibility in the selection of housing (example: "I only need a one-bedroom apartment but I would like to live in a neighborhood of single-family detached houses.")	

## E. NEIGHBORHOOD DESIGN

Citizens emphasized their desire for attractive, pleasant residential communities that would be compatible with existing neighborhoods, incorporate natural features, and provide adequate recreational opportunities. Residents in existing neighborhoods expressed concern over the impact of future commercial and office uses within their neighborhoods.

## F. RESIDENTIAL DEVELOPMENT ACTIONS

1. Retain the residential character of existing neighborhoods while providing for small scale office and commercial uses by permitting the adaptive reuse of existing residential structures. Require that non-residential uses be compatible with the residential character of the neighborhood.
2. Preserve the character of existing residential areas by maintaining the existing low density residential character of these areas. Permit more intensive residential development outside existing residential areas in order to encourage a mix of housing types serving a full range of interests, incomes and ages.
3. Except for existing residential areas, designate the Town Center as a Transfer Zone. Permit increased density with the use of Transferable Development Rights at a rate of five TDR's per additional unit within the Transfer Zone. Permit densities to be increased without purchasing TDRs for the purpose of providing housing for the elderly.
4. Permit clustering and mixed-density residential development in order to utilize the full development potential of a site and provide for a variety of housing types within any given site.
5. Establish regulations in the Zoning Ordinance governing the types of residential units that are to be permitted within specified areas of the Town Center. Base regulations on existing land use patterns and future housing needs.
6. Require that higher density development adjacent to existing neighborhoods be designed to be compatible with existing residential development in terms of height and scale.
7. Permit single-family detached residential development to meet the road construction and design standards of the Townhouse and Multi-family section of the Calvert County Zoning Ordinance. This will help reduce site construction costs, permit more flexibility in site design, and permit landscaping along roadways.
8. Require natural feature site inventories for the purpose of identifying significant trees and unique habitats which are to be considered in the preparation of subdivision designs.
9. Preserve the old town character of the residential area along Route 765 south of the Courthouse.
10. Promote good design to ensure privacy and space even at higher densities. In recent years, designers have developed housing types and styles that provide the financial benefits of higher densities with the benefits of privacy and space typical of single-family detached homes. All new residential development should ease its way into the existing fabric of the community by following the following guidelines:

continued next page...

a. Establish a suitable transition area between established residential neighborhoods and future residential developments. Development within the transition area should use the same setbacks, lot widths and housing types as in the immediately adjacent area.

b. Architectural designs that capture the flavor of old Prince Frederick should be encouraged.

c. Not all new residential areas need to use cul-de-sacs and curvilinear streets. A grid pattern of streets is equally appropriate within the Town Center and more in keeping with the character of old towns.

11. Provide guidelines for housing for low and moderate income families. The Comprehensive Plan calls upon the County to place special emphasis on providing housing opportunities for low and moderate income families who are residents of Calvert County by providing for increased dwelling unit density in the Town Centers. It is important to the well-being of the County as a whole that the housing needs of all its residents be served. It is also important not to greatly exceed the need and demand generated from within the County. To do so will place a burden on the financial resources of the County, resulting in either a need to increase taxes or reduce public services or both.

It is also important not to isolate or concentrate low and moderate income households into any one area of the Town Center, into any one Town Center or into any one district within the County. Instead, the County should strive for a balanced mix of housing types in each of its major Town Centers, including but not limited to Prince Frederick.

Note: Low and moderate income housing covers a wide range of housing categories. "Low income" is defined by HUD to equal 80% or less of the median income, depending on household size and is used as a means of determining qualifications for housing subsidies. "Moderate income" and "affordable housing" are general terms that usually refer to households that do not qualify for assistance but find themselves priced out of the housing market.



## V. ECONOMIC DEVELOPMENT

### KEY ISSUES:

**What can be done to persuade more County residents to shop in Prince Frederick?**

**What steps can the County take to attract businesses to Prince Frederick?**

**How can we promote economic development while still maintaining an adequate level of service on Route 2/4?**

### A. PURPOSE

Promoting economic development in the Town Center was one of the top priorities identified during the planning process. It is also one of the goals of the Comprehensive Plan. The most important step the County can take is to provide the public facilities needed to accommodate commercial and business growth. It is also important to provide as many reasons to visit the Town Center as possible. In Prince Frederick, the high concentration of public and quasi-public facilities and services, in particular, government services, helps attract new businesses and consumers to the Town Center. In addition, providing a more attractive setting for shoppers will help attract both consumers and investors.

### B. OBJECTIVES OF THE COMPREHENSIVE PLAN

In the Comprehensive Plan, the following objectives, listed under the economic development section, apply to commercial development in the Town Center:

1. Promote the economic health and expansion of existing local businesses.
2. Attract businesses and industries that provide job opportunities for the County's work force.
3. Encourage the development of businesses and industries which add stability and broaden the County's tax base.
4. Encourage and promote the development of industries which do not significantly contribute to environmental degradation.

### C. COMMERCIAL DEVELOPMENT AND FUTURE COMMERCIAL SPACE NEEDS

The Department of Planning and Zoning projects that the existing amount of office and commercial space will double by the year 2010. This estimate is conservative. Existing commercial and office development occupies an estimated 100 acres of land within the Town Center.

#### D. ECONOMIC DEVELOPMENT ACTIONS

1. Establish adequate water and sewerage facilities as top priority public improvement expenditures in the Prince Frederick Town Center in order to provide the basic infrastructure needed for economic development to occur.

2. The County should proceed immediately with the construction of additional roads within the Prince Frederick Town Center in order to provide an adequate road system and increase the amount of suitable space for new businesses.

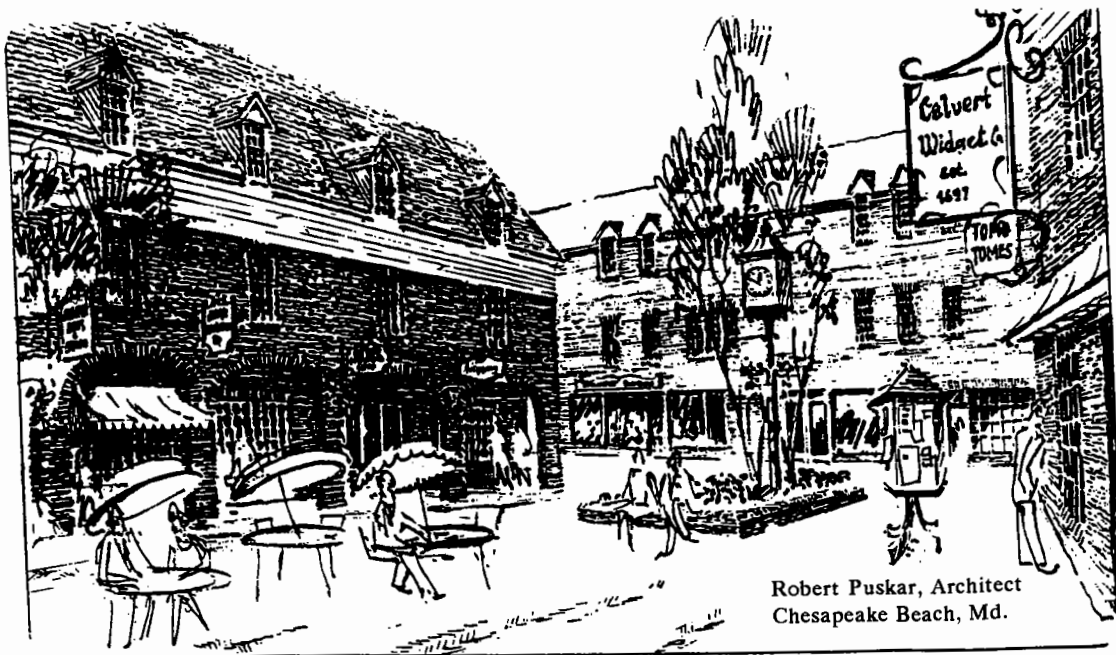
3. Continue to concentrate government services in old Prince Frederick within the immediate vicinity of the Courthouse in order to help maintain and expand the area as a center of business activity. Acquire additional land to provide for projected future government office space needs.

4. Establish an attractive image for Prince Frederick as a means of attracting investors, new businesses and consumers to the Prince Frederick Town Center. Budget an annual amount of general revenues to be used for providing the types of public amenities that will help attract investors, businesses and consumers to the Prince Frederick Town Center.

5. Conduct a study to determine the feasibility of developing regional storm water management facilities in the Town Center, such as lakes. This will help promote efficient utilization of land for new development.

6. Permit business directional signs at key intersections subject to established criteria and erect landscaped Town Center entrance signs, subject to regulatory approval, at the boundaries of the Town Center along Route 2/4 and Rte 231.

7. Identify areas that are most suitable for commercial uses and establish incentives and/or regulations governing permitted uses within these areas.



Robert Puskar, Architect  
Chesapeake Beach, Md.

## VI. TOWN CENTER APPEARANCE AND DESIGN

### KEY ISSUES:

**What can be done to give the Prince Frederick Town Center a positive identity?**

**What can be done to promote quality design?**

**How can we help create a cohesive Town Center design and keep the appearance of the Town Center from being fragmented?**

### A. INTRODUCTION

Towns are judged most frequently by their appearance. There is no other factor that can contribute as much to establishing clear guidance and direction for future growth than the appearance of a town. In Prince Frederick, it is the appearance of the oldest part of the town that most clearly expresses and defines its character.

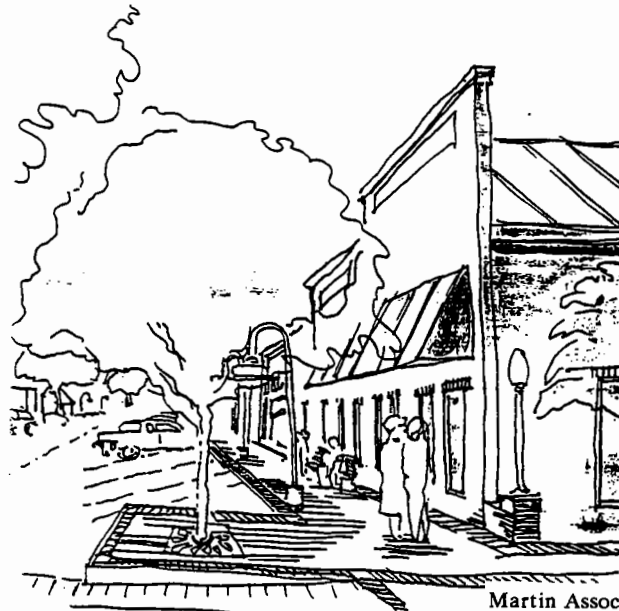
More recent development, which is located almost exclusively along Rte 2/4, is more diversified in terms of architecture and less cohesive in terms of overall appearance and design. These factors contribute to the impression of those traveling through town that Prince Frederick is little more than a typical commercial strip.

### B. BACKGROUND

Old Prince Frederick is a pleasant mix of office, residential, public and quasi-public buildings and spaces that together form a clearly identifiable pattern of development. The historic character of much of the architecture of the area, together with its density and scale, provides an aesthetically pleasing streetscape.

Design standards and appearance guidelines can help retain the existing character of the old town and contribute to a greater sense of identity and a more attractive appearance for the Prince Frederick Town Center overall.

Old Prince Frederick naturally evolved as a mixed-use zone, as have many other attractive small towns. Mixed-use zoning provides maximum flexibility for proposed new developments. Residential, public, commercial and office uses can share the same space and even the same building. The entire Prince Frederick Town Center is a "mixed-use" zone.



Martin Associates, Architects  
Washington, D.C.

## C. APPEARANCE AND DESIGN ACTIONS

1. Adopt an Appearance Code for the Town Center. The Appearance Code should cover the rehabilitation and maintenance of existing buildings and new construction. Topics to be addressed in the Appearance Code should include: Building Form, Facade Treatment, Scale, Landscaping, Signs, Open Space, Pedestrian Circulation, Street Amenities, Setbacks and Procedure for Design Review. The purpose of the Appearance Code should be to:

a. foster a positive identity for the Town Center

b. establish standards for signs that will minimize clutter and promote an attractive appearance along roadways.

c. establish parking lot standards that will reduce the visual impact of large expanses of pavement and require a careful analysis of on-site and off-site vehicular and pedestrian circulation and the impact of parking facilities on the overall land use pattern.

d. require consideration of pedestrian circulation among sites and provide a standard for sidewalk design. Develop a sidewalk ordinance governing design, construction and maintenance.

e. require that entrances to commercial developments are designed to promote visibility, identity and safety.

f. require that landscaping be designed to accent buildings, entryways and signs and draw attention away from parking lots, utility lines and outdoor storage areas.

g. require an analysis of views and view corridors

h. require that buildings be well oriented to roads and that the typical linear strip commercial development pattern be avoided or mitigated through appropriate site design.

i. require an analysis of architectural relationships with surrounding buildings, including building siting, massing, facade treatment, materials, proportion, scale, color and maintenance of streetscape.

j. require an analysis of existing natural features of the site and planned open space and show how that open space can function as a pedestrian and architectural amenity in the Town Center.

2. For the purposes of the Appearance Code, establish boundaries for an Old Prince Frederick District based upon the Prince Frederick Historic Sites Survey (1986). Appearance Code guidelines for development within the Old Prince Frederick District should be based upon the predominant architectural styles and existing development patterns within the old town. Note: The Old Prince Frederick District should not be equated with a Historic District which has far more stringent design requirements.

3. Outside the boundaries of the Old Prince Frederick District, Appearance Code guidelines should promote designs that blend regional historical building materials and building forms with a more contemporary image.

4. Appoint an Appearance Review Committee to advise on the implementation of the provisions of the Appearance Code. Area garden clubs, residents and business owners should be represented on the Committee.

5. Following adoption of the Master Plan, the Planning Commission shall adopt an interim set of guidelines to be used in assisting property owners in the design of proposed projects. These guidelines shall reflect the intent of the community to promote a clear identity for the Town Center in accordance with the guidelines stated above and will be incorporated into the Appearance Code.

continued next page....

6. Encourage owners of properties that have been identified in the Calvert County Historic Sites Survey to participate in the Calvert County Historic District Program and to renovate rather than tear down existing older structures. If a building is listed on the Calvert County Historic Inventory, the owner should be requested to inform the Calvert County Historic District Commission of any plans to tear down the building, giving the Commission an opportunity to document the building and/or look for alternatives that can be used to save the building, if deemed desirable. Require archeological inventories as part of the site plan review process for major sites within the Old Prince Frederick District.

7. Develop a unified landscaping plan to be developed and implemented along the Rte 2/4 right-of-way and median strip.

8. Reserve space for underground utilities along all new roadways. Require that all new utility lines be put underground. Develop a long range plan in cooperation with utility companies to bury utility wires and/or relocate poles.

9. Existing traffic signal wires and wooden poles should be redesigned to improve the appearance of Rte. 2/4. New traffic signals should be required to match the selected new style.

10. A comprehensive network of sidewalks should be designed for the Old Prince Frederick District. At a minimum, a sidewalk should be constructed along Main Street from the southern boundary of the Town Center to Church Street and along Church Street to Rte. 2/4. A standard for sidewalk design should be developed.

11. Public parking lots should be provided in the Old Prince Frederick District and County requirements should be modified to permit more extensive use of shared parking lots in order to maintain the traditional closely spaced development pattern of old towns.

12. Street lights should be provided along Church Street, Main Street and Duke Street. The County should evaluate the desirability of providing additional street lights along public roadways based on an assessment of installation and operating costs.

The County should work with business and community groups to select compatible street light designs for use in the Old Prince Frederick District and along Rte 2/4 and secondary roads.

The County should require that if street lights are provided in new developments that they be the same as or compatible with any public street lights provided.

13. The County, in conjunction with area civic groups and business associations, should present awards for outstanding contributions to the design and appearance of the Prince Frederick Town Center.



Robert Puskar, Architect  
Chesapeake Beach, Md.



## VII. PARKS, OPEN SPACE AND SPECIAL DESIGN PROJECTS

### KEY ISSUES:

**What areas should be preserved as open space?**

**Where should trails be planned?**

**Where could public meeting spaces be developed?**

### A. COMPREHENSIVE PLAN

The Comprehensive Plan calls for recreational open space to be provided within Town Centers. It points out that public squares were an integral feature of towns built in the 18th century. It calls for this kind of open space to be at or near centers of activity.

The Comprehensive Plan also recommends providing safe bicycle travel within towns and convenient pedestrian access.

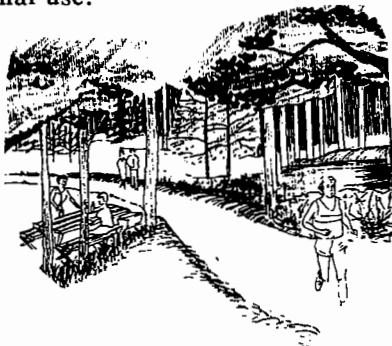
### B. EXISTING SITUATION

#### 1. Natural Areas

Stream valleys and steep slopes surround Prince Frederick, providing a natural "green belt". They also intersect the Town Center, helping to create a pattern of small clusters of development surrounded by open space. As the Town Center grows, these natural areas will become increasingly important not only for their ability to absorb pollutants, control run-off and moderate the climate, but also for their ability to diminish the sense of crowding that is a typical response to rapid growth.

#### 2. Individual Sites Within the Town Center

In addition to the protection of natural areas and their potential for recreation, there are several sites within the Town Center that should be developed and improved for recreational use.



#### a. The Courthouse Grounds

The Courthouse provides a strong focal point for old Prince Frederick. The Courthouse lawn, while very attractive, does not function well as a public gathering space. A public open space with landscaping and benches within the immediate vicinity of the Courthouse would provide space for ceremonies, outdoor art festivals, farmers' markets and concerts, bringing more activity into the area, particularly in the evenings and on weekends.

#### b. The Intersection of Duke Street and Main Street

The intersection of Main Street and Duke Street is an important intersection because of its proximity to the Courthouse and because it is an obvious focal point that can be seen clearly from anywhere along Duke Street and large portions of Main Street. At present, however, the intersection lacks definition and a sense of place. A small landscaped area, incorporating the Wisteria as a major feature, can provide an attractive focus for the Old Prince Frederick District.

#### c. Calvert Middle School

The Calvert Middle School is a significant community symbol. It could provide a strong visual focus for this part of town. The chain link fence along Route 2/4 detracts from the intersection. Painting the fence black or dark green would reduce its visual impact. Planting along the perimeter would substantially improve the appearance of the site.

d. Old School on Armory Road

The old school on Armory Road should be renovated and preserved. The County and the Board of Education should encourage proposals for adaptive re-use.

e. Potential Lake Sites

While land is still available, consideration should be given to creating a lake or lakes within the Prince Frederick Town Center at the headwaters of one or several creeks. Attractive parks with nature trails, bikeways and picnic benches could be developed around such lakes and connected to built-up areas by walkways. One or several man-made lakes would not only provide recreational and aesthetic benefits, but would also serve as stormwater detention ponds.

f. Trails

The old railroad bed, the old Parker's Creek road bed and the major stream valleys which drain into Parker's Creek, Hunting Creek and Battle Creek should be considered as possible areas to construct trails.

g. Roads

Develop a new road cross-section for use in the Old Prince Frederick District in order to ensure that new roads will be compatible in terms of design, scale and landscaping with the existing character of the old town.

h. Neighborhood Recreation

The demand for additional public recreational open space can be offset by requiring that new residential neighborhoods provide their own recreational open space.

**C. PARKS, OPEN SPACE AND SPECIAL PROJECTS ACTIONS**

1. Existing forests, including significant trees and groves should be retained to the greatest extent possible and incorporated into future site design. Grading permit procedures shall be amended to require that no grading or clearing permits for clearing sites shall be issued until a site plan or subdivision plan where required is approved by the Planning Commission.

2. Provide a public open space near the Courthouse to be used for ceremonies, outdoor art festivals, concerts and farmers' markets.

3. Make improvements to the intersection of Duke Street and Main Street to give it more definition.

4. Create a small landscaped area around the Wisteria next to High's as an attractive focus for the Old Prince Frederick District.

5. Take the following actions to enhance the site of the Calvert Middle School:

a. Paint the chain link fence black or dark green.

b. Encourage civic groups to provide landscaping.

6. The old school site on Armory Road should be renovated and preserved and proposals for use by County agencies or other groups should be entertained.

7. A study should be conducted to determine the comparative cost and feasibility of creating lakes that can serve the dual function of providing recreational assets and regional stormwater management systems. Prime sites to consider are the area behind the future State Office Building and the area immediately south of the hospital property.

8. Continue to require that new residential neighborhoods provide their own neighborhood recreation areas in order to ensure that children will have a safe place to play close to home and to provide a focus for community interaction. Developers should be required to provide interconnecting sidewalks between adjacent residential neighborhoods.

In some cases, fees in lieu of providing neighborhood recreational space may be appropriate, but this is an issue that needs to be addressed on a case-by-case basis. Any fees collected in Prince Frederick should be used to provide recreational open space in the Town Center.

## VIII. TRANSPORTATION

### KEY ISSUES:

**How can problems at specific intersections be resolved?**

**How can the flow of traffic on Route 2/4 be maintained?**

**How many entrances to Route 2/4 should be allowed and where should they be?**

**What new roads, if any, are needed?**

### A. OBJECTIVES AND RECOMMENDATIONS IN THE COMPREHENSIVE PLAN

In the Transportation Section of the Comprehensive Plan, these objectives are relevant to the Prince Frederick Town Center:

1. Stage the development of a transportation system to complement the overall development of the County.
2. Maintain Routes 2 and 4 as the main transportation corridor providing for safe and efficient travel.
3. Encourage transportation alternatives such as public transit, car pools, bikeways and pedestrian ways which reduce the dependency on individual automobiles.

The following recommendations in the Comprehensive Plan have a bearing on the Prince Frederick Town Center:

1. Prohibit ingress-egress points on Route 4 wherever possible.
2. Limit major intersections to essential locations such as access to designated Town Centers and collector roads.
3. Develop and implement a landscaping program for the Route 2/4 corridor by enlisting cooperation from the State Department of Transportation and private interests.

4. Implement a program of spot improvements, aimed at the needs of particular intersections and road segments.

5. Establish bicycle and pedestrian routes to connect residential, employment, educational, recreational and open space areas, as feasible.

6. Require sidewalks in areas where there is high pedestrian and vehicle traffic in apartment areas and commercial areas.

7. Consider providing sidewalks in established commercial areas such as Solomons, St. Leonard, Old Prince Frederick and Owings.

These objectives and recommendations were taken into consideration in developing this Plan.

### B. TRAFFIC CIRCULATION STUDY

In March of 1987, a traffic consultant was hired to develop a comprehensive, long-range program applicable to the year 2010 which would effectively maintain safe, free-flowing traffic along Route 2/4.

The recommendations in this Plan and the Road System Map (shown in the Prince Frederick Zoning Ordinance) are based on the traffic consultant's report but have been modified to take into consideration testimony presented at the public hearings.

### C. EXISTING SITUATION

The road system in Prince Frederick is currently operating at excellent to good levels of service according to national standards. However, the following problem areas have been identified:

#### 1. Signalized Intersections

a. The Dares Beach/Route 2/4 intersection is becoming increasingly congested at peak hours.

b. The Route 231/Route 2/4 intersection is not designed to handle a substantial increase in traffic. It would be difficult to improve it because of topographic and alignment constraints.

#### 2. Non-signalized Intersections and Crossovers

a. The Armory Road/Route 2/4 intersection near McDonald's is inadequately designed to handle the existing and projected traffic in this area. Signalization or intersection redesign will be required within the next several years.

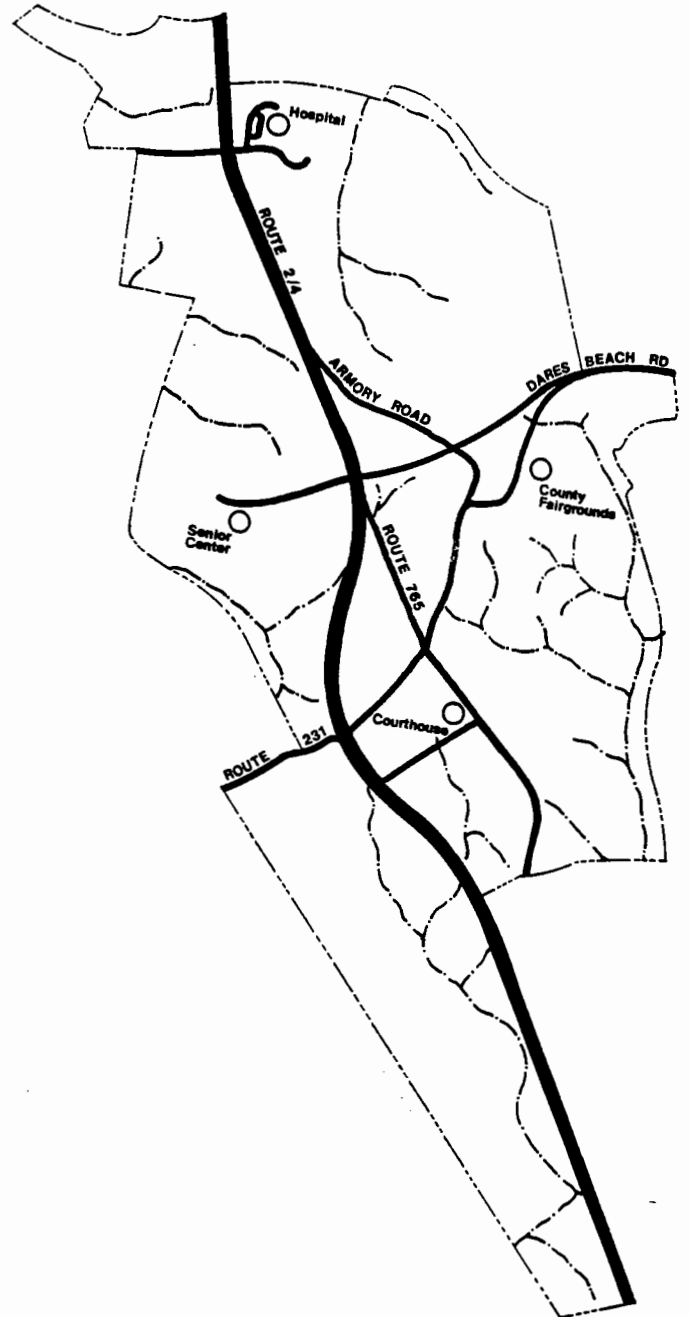
b. The Dares Beach Road/Armory Road intersection is poorly aligned.

c. The Route 765/Church Street split intersection is likely to become a problem as traffic increases.

d. The Duke Street/Route 2/4 intersection has a narrow median which gives inadequate protection to automobiles waiting to make left turns.

e. The Route 2/4/Route 765 intersection at the southern boundary of the Town Center will require improvements if traffic increases substantially in this area.

f. The crossover on Route 2/4 immediately to the south of Dares Beach Road is too close to the intersection. As traffic increases, there will be a stacking problem created by cars trying to enter Route 765 and a weaving problem as cars enter Route 2/4 from Route 765 and cross lanes to make a left turn at the Dares Beach Road intersection.



### 3. Route 2/4

As Figure 1 indicates, rush hour traffic on Route 2/4 is well below the capacity of the highway at the present time. Capacity refers to the maximum volume of traffic that can be accommodated on a roadway. Capacity is strongly affected by the number of driveway entrances, crossovers and intersections along a roadway. A restricted access highway can carry high volumes of traffic. As the number of driveway entrances, crossovers and intersections increases, the capacity of the highway decreases. Therefore, determining the number and location of driveway entrances, crossovers and intersections along Route 2/4 is a major task of this master plan.

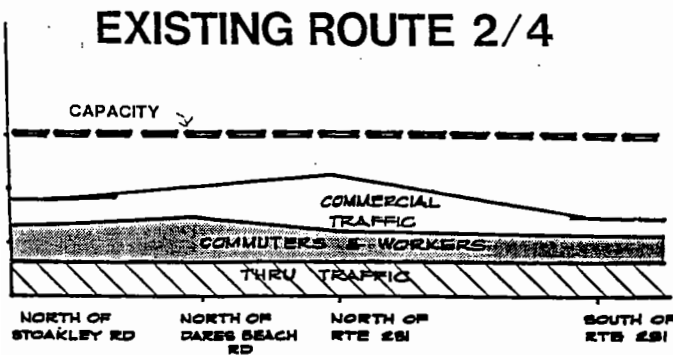


FIGURE 1

### 4. Projected Traffic Impact

Traffic projections were developed on the basis of dwelling unit and commercial development projections. They indicate that traffic on Route 2/4 will more than double between 1986 and 1995 between Stoakley Road and Duke Street.

Figure 2 shows that prior to the year 2000, Route 2/4 will be heavily congested from Stoakley Road to Route 231 (Hallowing Point Road). (This graph assumes the present roadway system and a lack of adequate access controls.)

Route 2/4 will have reached an F (failed) level of service prior to the year 2000, assuming no change to the current road network and the absence of access controls. Up to eight traffic signals will be located along the three-mile stretch of highway within the Town Center if no access controls are implemented. The most serious concern is that Route 2/4 is the only route to the Calvert County hospital and the only access route for fire and rescue vehicles and police. It is also the major evacuation route in the County.

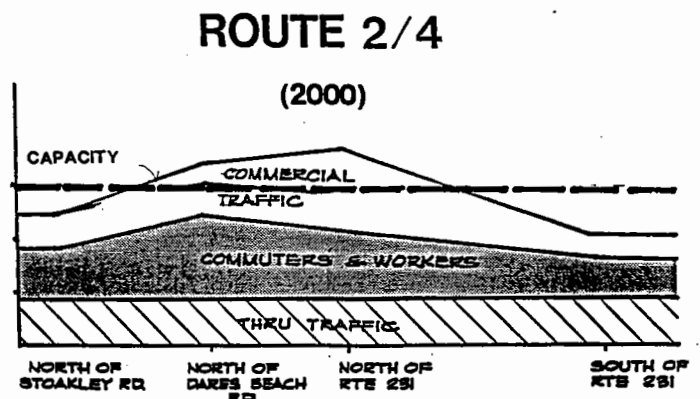


FIGURE 2

D. OPTIONS

The following traffic management options for Route 2/4 have been considered:

1. OPTION #1 CONSTRUCT A BY-PASS. A by-pass located outside the Town Center would divert all through traffic away from Route 2/4.

Advantages	Disadvantages
a. It would provide fast, convenient travel for commuters and tourists on their way to Solomons and points south. This would only be true if the by-pass were designed and maintained as a limited access highway.	a. If a new by-pass is constructed, it will have to intersect major stream valleys and steep slopes resulting in extremely high road construction costs and potential damage to the environment.
b. It would maintain the function of Route 2/4 to the year 2005. After 2005, travelers on Route 2/4 would face the same problems described in Section C4 above.	b. Existing farms and residential communities outside Prince Frederick will be negatively impacted by a by-pass.
c. It would provide an alternative access route for emergency vehicles and evacuations.	c. A by-pass would be a major disincentive to new commercial development in Prince Frederick.

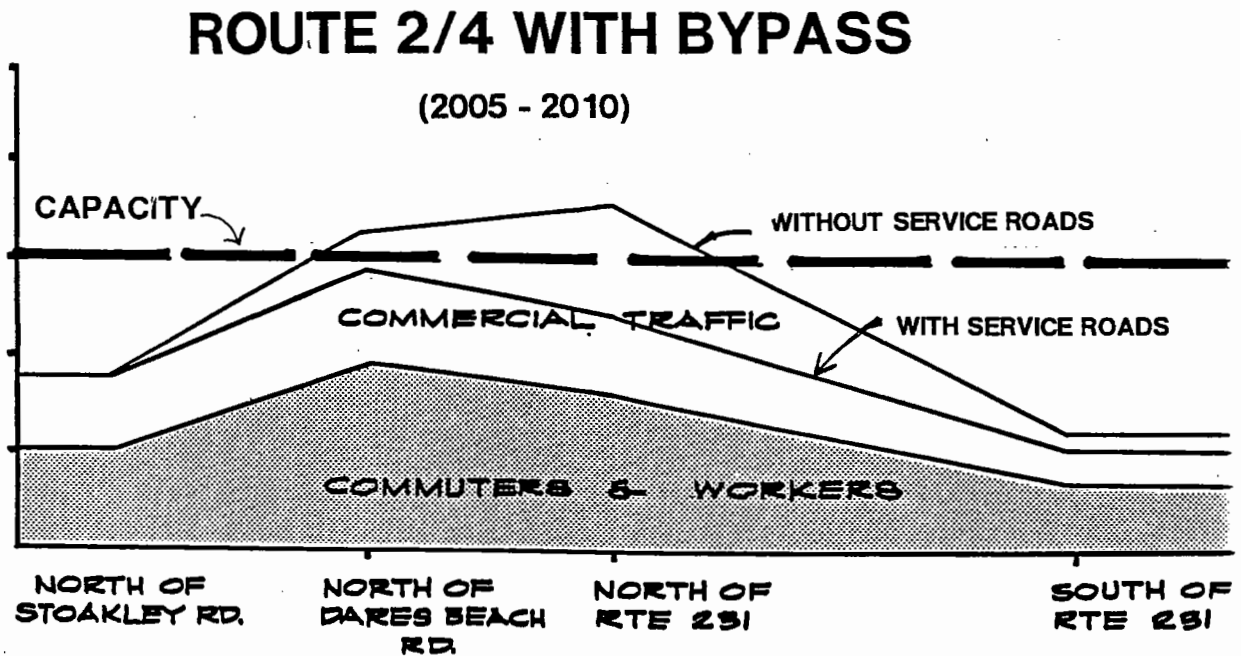
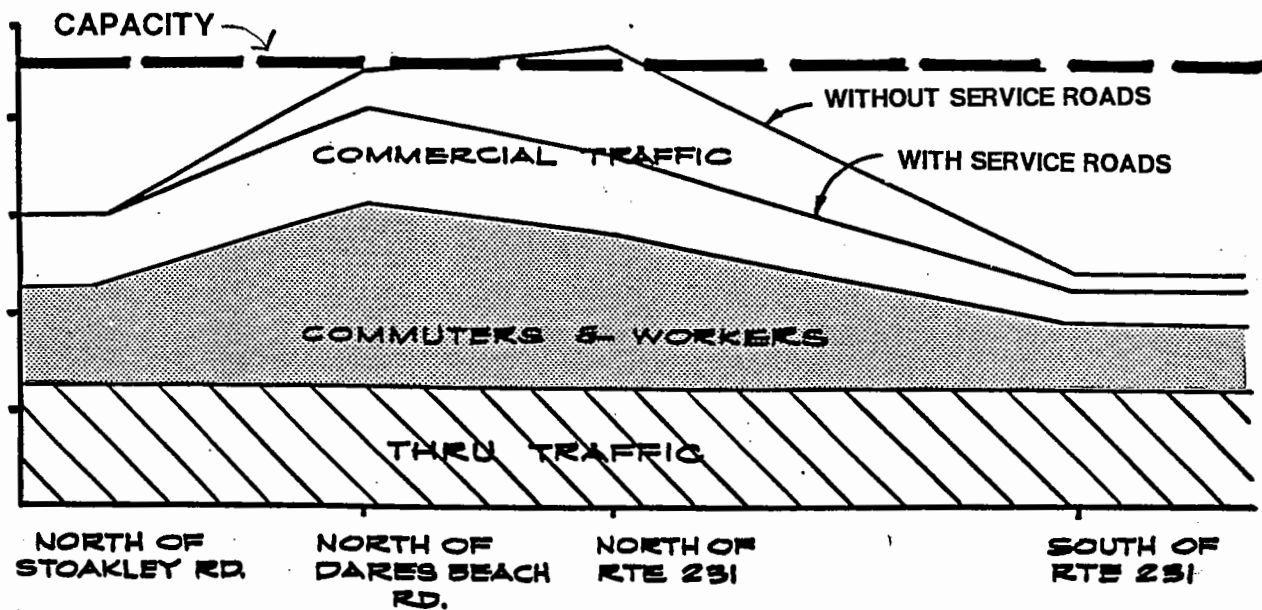


FIGURE 3

2. OPTION #2 WIDEN ROUTE 2/4 TO SIX LANES.

Advantages	Disadvantages
a. Route 2/4 would continue to function at adequate levels of service to the year 2010 (approximately).	a. Some existing businesses could be required to relocate to make room for the widened right-of-way.
b. The cost of widening would be borne by the State. As with a by-pass, the State would have to agree to widen the highway, acquire funding and schedule the project.	b. Some undeveloped parcels along Route 2/4 could lose some of their development potential through the loss of land area needed for the right-of-way.
	c. Travel along a six-lane highway is more confusing and hectic, a factor that could affect elderly residents in particular by diminishing their mobility.
	d. Widening Route 2/4 without access controls will not in any way reduce the proliferation of driveway entrances and signalized intersections.

## WIDENING ROUTE 2/4



**FIGURE 4**

3. OPTION #3 DEVELOP AN INTERCONNECTED ROADWAY SYSTEM IN PRINCE FREDERICK TOGETHER WITH ACCESS CONTROLS TO RELIEVE PRESSURE ON ROUTE 2/4.

Advantages	Disadvantages
<p>a. The highway would continue to function at adequate levels of service to the year 2000.</p>	<p>a. The cost of new road construction would have to be borne by taxpayers, developers and business owners. It is estimated that the cost of constructing new roads would average \$1,000,000 a mile (current dollars) for a total of three to four miles. Costs could substantially exceed these estimates in areas of particularly rough terrain.</p>
<p>b. New local roads can provide space for more businesses. There are many types of businesses that can not afford a Route 2/4 location or the rents in shopping centers.</p>	<p>b. New roads and access controls will not eliminate existing traffic signals and will require up to three additional traffic signals. This will require a reduction in travel speed through Prince Frederick and increase travel time for commuters.</p>
<p>c. Additional roads would provide alternate routes for emergency vehicles in the event of major traffic congestion on Route 2/4.</p>	<p>c. Stricter access controls will affect potential commercial developments on Route 2/4 by limiting their access to the highway. This could be a disincentive to new development at some locations.</p>
<p>d. New local roads can help divert traffic from existing residential neighborhoods. Residents on Armory and Fairgrounds Roads are already experiencing substantial new traffic generated by people who use these roads to "short cut" the highway and avoid traffic signals.</p>	<p>d. Stricter access controls may cause a decrease in sales for some existing businesses located along Route 2/4. The degree of negative impact, if any, would be strongly influenced by the locations of new roads. If these roads are located to provide dual access (from the highway and the local road), the impact is not likely to be severe and could be beneficial.</p>
<p>e. Residents who are not comfortable with driving on a major highway would be able to reach stores, services and community activities by traveling on slower, less hectic local roads.</p>	

**OPTION # 4 ESTABLISH ROUTE 2/4 AS A LIMITED ACCESS HIGHWAY. PROVIDE ONE ENTRANCE TO THE BUSINESS DISTRICT AT THE NORTHERN END OF THE TOWN CENTER AND ONE ENTRANCE AT THE SOUTHERN END. CONSTRUCT AN UNDERPASS AT THE INTERSECTION OF ROUTE 2/4 AND ROUTE 231 (HALLOWING POINT ROAD)<sup>1</sup>**

Advantages	Disadvantages
a. This would have a less severe impact on businesses than developing a by-pass outside the Town Center. Route 2/4 would function as a by-pass (as originally intended) while maintaining visibility for local businesses.	a. Existing businesses would lose sales generated by through traffic. This would be a less severe loss than if a by-pass were to be constructed.
b. The cost of converting Route 2/4 to a limited access highway would be significantly less than constructing a new by-pass.	b. It could discourage new businesses from locating in Prince Frederick, particularly those oriented to through traffic trade.
c. This approach could postpone or eliminate the need to widen Route 2/4 and would probably eliminate the need for a future by-pass while maintaining adequate levels of service on Route 2/4.	c. Church Street would need to be upgraded to accommodate heavy traffic. This would significantly change the character of the street and generate a demand to permit highway oriented uses along Church Street.
d. The cost of buying up existing driveway entrances along Route 2/4 and constructing an underpass could be funded in whole or in part by the State.	
e. Existing and future traffic signals along Route 2/4 would be eliminated.	
f. Residents would be able to reach stores, services and community activities without traveling on the highway.	
g. An underpass would eliminate the existing traffic problems at the Route 231 intersection.	
h. Traffic would be funneled toward old Prince Frederick, creating a potential for business expansion.	

<sup>1</sup>Submitted by County residents.

**OPTION #5 DESIGN NEW ROADS TO FUNCTION AS LIMITED ACCESS ROADWAYS. DO NOT RESTRICT ACCESS ON ROUTE 2/4.<sup>2</sup>**

Advantages	Disadvantages
a. Existing and future businesses would have direct access to the highway.	a. It is not clear where such new limited access roadways could be built.
b. Commuters and tourists would have fast, convenient travelways.	b. The cost of road construction would probably have to be borne by local taxpayers and would be higher than the cost of constructing local roads.
c. The cost of constructing limited access roads in Prince Frederick could be less than the cost of constructing a by-pass at a more distant location and would serve a similar function.	c. Businesses along Route 2/4 would lose sales generated by through travelers but not to the same extent as in the by-pass alternative.
d. There would be less disruption to existing households than in the by-pass alternative.	d. Without access controls, Route 2/4 would fail by the year 2005. With access controls, the highway could continue to function beyond 2010.
e. The distance between the limited access roads and Route 2/4 would help reduce the potential loss in sales generated by through travelers.	

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<sup>2</sup>Submitted by County residents

## E. ACTIONS

Clearly, there are no easy solutions to the problem of future traffic congestion on Route 2/4. However, despite the disadvantages of all of the options, doing nothing would have even more disadvantages. Since the traffic consultants' report was completed, traffic counts show that traffic volumes are increasing at a much higher rate than projected. This factor indicates that it is imperative from the standpoint of traffic safety and economic development that the County act immediately to upgrade the roadway system in accordance with the provisions listed below.

The following actions are a combination and modification of the options designed to best meet the transportation needs of the Town Center.

1. Adopt the Road System Plan (shown in the Prince Frederick Zoning Ordinance). This plan represents a general concept. Actual future road locations will be based on detailed engineering studies and should be designed in conjunction with the space requirements of individual development projects.

2. Adopt the following access control policies:

a. No more than one traffic signal and four-way intersection mid-way between Stoakley Road and Dares Beach Road may be permitted subject to SHA approval.

b. No more than one traffic signal may be permitted between Dares Beach Road and Rte 231 subject to SHA approval.

c. A traffic signal may be permitted in front of the Prince Frederick Fire Station subject to SHA approval.

d. No use or combination of new or existing uses that will generate the need for a traffic signal in locations other than those listed above will be permitted on Rte. 2/4 within the boundaries of the Town Center.

e. No additional crossovers will be permitted in the Town Center except at Commerce Lane and the Fire Station.

f. Driveway entrances and/or roads may not align with crossovers except as indicated on Road System Plan.

g. In order to minimize the number of new driveway entrances along Rte 2/4, only one driveway entrance may be permitted per existing parcel of record. In cases where a site can be accessed by a secondary road instead of a new driveway entrance, access shall be by way of the secondary road.

3. Work with the State Highway Administration (SHA) to closely monitor the need for widening Rte 2/4 to six lanes from Stoakley Road to the fire station and take action accordingly.

4. Request SHA to begin preliminary work on locating a potential route for a future by-pass.

See Chapter IX for information about financing the road network.

5. New roads and intersection improvements should be designed, where feasible, to avoid directing through traffic onto roads which serve existing residential neighborhoods.

6. Include bicycle lanes in the design and construction of selected new local roads, where feasible.



## F. Road Improvement Plan

Calvert County and SHA, working in conjunction, should:

1. Construct a secondary (service) road from Stoakley Road to Rte 231 on the west side of Rte 2/4. The segment between Dares Beach Road and Rte 231 should be priority one.
2. Construct a secondary (service) road on the east side of Rte 2/4 from Armory Road to the north boundary of the Town Center.
3. Realign Armory Road to connect to service road. Redesign crossover at Armory Road and Rte 2/4 to limit number of permitted turning movements.
4. Realign Armory Road south of Dares Beach Road and provide remedial treatment if warranted.
5. Construct crossover at Commerce Lane and realign entrance. Subject to an assessment of traffic safety and the construction of a crossover of Commerce Lane, cul-de-sac the existing connector with Rte 2/4 and Rte 765, subject to SHA approval.
6. Realign intersection at Armory Road, Church Street and Rte 765.
7. Eliminate existing crossovers or limit the number of permitted turning movements at crossovers.
8. Design a system of crossovers in the immediate vicinity of the Fire Station to help alleviate intersection problem at Duke Street while maintaining rapid ingress and egress for volunteers responding to alarms.
9. Improve intersection at Rte 765 and Rte 2/4 at the southern boundary of the Town Center.
10. Assess road and intersection situation on Fairgrounds Road and take measures as needed to restrict traffic through the residential area.

11. Extend Duke Street to serve State Office Building if necessary.

12. Upgrade the intersection at Rte 2/4 and Rte 231 as needed.

Additional roads and/or modifications to this Improvement Plan may be approved to meet traffic safety needs as determined by the State Highway Administration and the County Engineer.

IX. IMPLEMENTATION AND FINANCING

KEY ISSUES:

**Which projects should be implemented first?**

**How much will each project cost?**

**Which agency will be responsible for seeing that a project is carried out?**

**How should each public improvement be financed?**

A. PUBLIC IMPROVEMENTS

The public improvements identified in the Prince Frederick Town Center Master Plan should be implemented in the following order of priority:

1. Upgrade Prince Frederick water system to meet fire prevention requirements.

Agency: Department of Public Facilities, Division of Water and Sewerage.

Estimated Cost: \$630,000

2. Construct Land Treatment Wastewater System

Agency: Department of Public Facilities, Division of Water and Sewerage.

Estimated Cost: \$5,700,000

3. Acquire right-of-ways and construct primary segments of the proposed new road network.

Agency: Department of Planning and Zoning and Department of Engineering

Estimated Cost: Construction: \$1,000,000 per mile; right-of-way acquisition to be determined on a case-by-case basis.

4. Design and construct a public plaza and unified landscaped area within the immediate vicinity of the Courthouse.

Use the Wisteria as a design element together with additional landscaping, seating areas and walkways. This project should be developed in conjunction with the proposed construction of a State office building.

Department of Planning & Zoning and Department of Engineering

Estimated Cost: to be determined

5. Construct a sidewalk along Main Street from Town Center boundary to the intersection of Church Street and along Church Street to Route 2/4.

Agency: Construction and maintenance - Department of Engineering. Acquire easements and require private construction as development occurs - Planning Commission and the Department of Planning and Zoning.

Estimated Cost: Town Center Boundary to Church Street: \$163,900  
Church Street to Rte 2/4: \$91,000

6. Fund a demonstration project to provide street lights between Old Field Inn and Rte 231. Evaluate the costs associated with the project and determine whether to expand the street light system along Church Street, Duke Street and the remainder of Main Street.

The Appearance Review Committee should select a street light design which is compatible with the historic character of the area.

Agency: Installation and Maintenance - Department of Engineering. Require installation as development occurs - Planning Commission and Department of Planning and Zoning.

Estimated Cost: Old Field Inn to Rte 231: \$99,000

7. Curb and landscape the perimeter of the parking lot at the site of the Evans Hotel.

Agency: Department of Planning and Zoning and Department of Engineering.

Estimated Cost: \$25,600

8. Provide landscaped Prince Frederick Town Center entrance signs at either end of the Town Center on Route 2/4.

Agency: Department of Administration and Finance:

Estimated Cost: \$9,000 per sign

9. Construct a trail system connecting Prince Frederick with natural areas outside the Town Center subject to a study and approval of property owners.

Agency: Acquire easements as development occurs: Planning Commission and Department of Planning and Zoning. Construct and maintain trail system: Department of Public Facilities and Department of Engineering.

Estimated Cost: \$5.00 to \$10.00 per lineal foot

10. Construct a lake or lakes and surrounding walkway(s) within the Town Center, subject to feasibility study.

Agency: Maryland Soil Conservation Service, Department of Planning and Zoning, and Department of Engineering.

Estimated Cost: Feasibility study: \$25,000

11. Install landscaping along median and street lights at intersections along the Route 2/4 corridor.

Agency: Installation and Maintenance: Department of Public Facilities. Require installation as development occurs: Planning Commission and Department of Planning and Zoning.

Estimated Cost: Demonstration Project: \$10,000

12. Replace existing traffic signal wires and wooden poles with poles the design of which is to be approved by the Planning Commission.

Agency: Department of Planning and Zoning and the State Highway Administration.

Estimated Cost: \$50,000 - \$75,000 per intersection

## B. FINANCING

One of the shortcomings of most plans is that proper financial steps are not taken to assure implementation. It is proposed that public improvements called for in this plan be financed through the Capital Improvement Plan and Budget including the use of a transfer tax, special taxing districts and the use of grants. A minimum of \$300,000 in general revenues should be set aside each year to help finance roads and public improvements within the Town Center. General revenue bonds which involve long-term payments on loans should be strongly considered.

Some projects, such as landscaping, could be financed through donations from civic groups and businesses and/or annual assessments based on the cost of landscape maintenance. Both on-site and off-site improvements associated with new developments will continue to be required.

It is reasonable that the expenses for public improvements in the Prince Frederick Town Center be paid for out of general revenue because Prince Frederick is the County Seat. The economic and social benefits to the whole County would compensate for the cost of the improvements. There are other options available to the County. However, they have some disadvantages, which should be carefully considered before using any of them. Options for financing new roads are listed below:

1. For proposed developments that are located within staging areas designated for road construction, enact one or more of the following financing mechanisms:

a. payment of road frontage fees for all new development fronting on a portion of the new roadway.

b. a road taxing district for properties fronting on the roadway. This approach would spread the payment over a longer period of time.

c. general revenue bonds. This approach would allow a long term pay back period financed in whole or in part through the increased revenues generated by new businesses that locate in Prince Frederick.

2. For proposed developments that are located within a staging area that is not designated for road construction at the time:

a. Permit the project on the condition that the cost of the roadway segment, defined as the area between intersections, will be borne by the project developer. In this event, road fees will not be required.

b. Require the dedication of a road right-of-way as part of site plan or subdivision approval for properties located within the area of development and require either rough grading of the road bed where appropriate or road construction where needed to serve the development.

**PRINCE FREDERICK  
ZONING ORDINANCE**

RESOLUTION NO. 8-92

PERTAINING TO THE ADOPTION OF THE PRINCE FREDERICK ZONING ORDINANCE.

WHEREAS, pursuant to the authority contained in Article 66B of the Annotated Code of Maryland, the Board of County Commissioners of Calvert County, Maryland has general powers to promulgate zoning ordinances; and

WHEREAS, after due notice was published, the Planning Commission and the Board of County Commissioners conducted a joint public hearing on October 8, 1991 at which time the proposed Prince Frederick Zoning Ordinance was discussed and public comment was heard; and

WHEREAS, on November 12, 1991 the Planning Commission approved the Zoning Ordinance and recommended adoption of the same to the Board of County Commissioners; and

WHEREAS, on January 7, 1992 the Board of County Commissioners unanimously adopted the Prince Frederick Zoning Ordinance without substantive changes.

NOW, THEREFORE, BE IT RESOLVED AND ORDAINED, by the Board of County Commissioners of Calvert County, Maryland, that the Prince Frederick Zoning Ordinance which is attached hereto as Exhibit A and made a part hereof, BE and hereby IS, adopted as the official Zoning Ordinance for the Prince Frederick Town Center.

AND BE IT FURTHER RESOLVED, that this Resolution and Ordinance shall be in force and effect from and after its adoption on the date set forth below.

DONE, this 28<sup>th</sup> day of January, 1992, by the Board of  
County Commissioners of Calvert County, Maryland, sitting in regular session.

ATTEST:

BOARD OF COUNTY COMMISSIONERS  
OF CALVERT COUNTY, MARYLAND

Mary S. Watson  
Mary S. Watson, Clerk

Michael J. Moore  
Michael J. Moore, President

Hagner R. Mister  
Hagner R. Mister, Vice President

Approved for legal  
sufficiency on 1-22-92  
by

Patrick M. Buehler  
Patrick M. Buehler

William R. Bailey  
William R. Bailey  
County Attorney

Mary M. Krug  
Mary M. Krug

Joyce Lyons Terhes

ff 1/21/92

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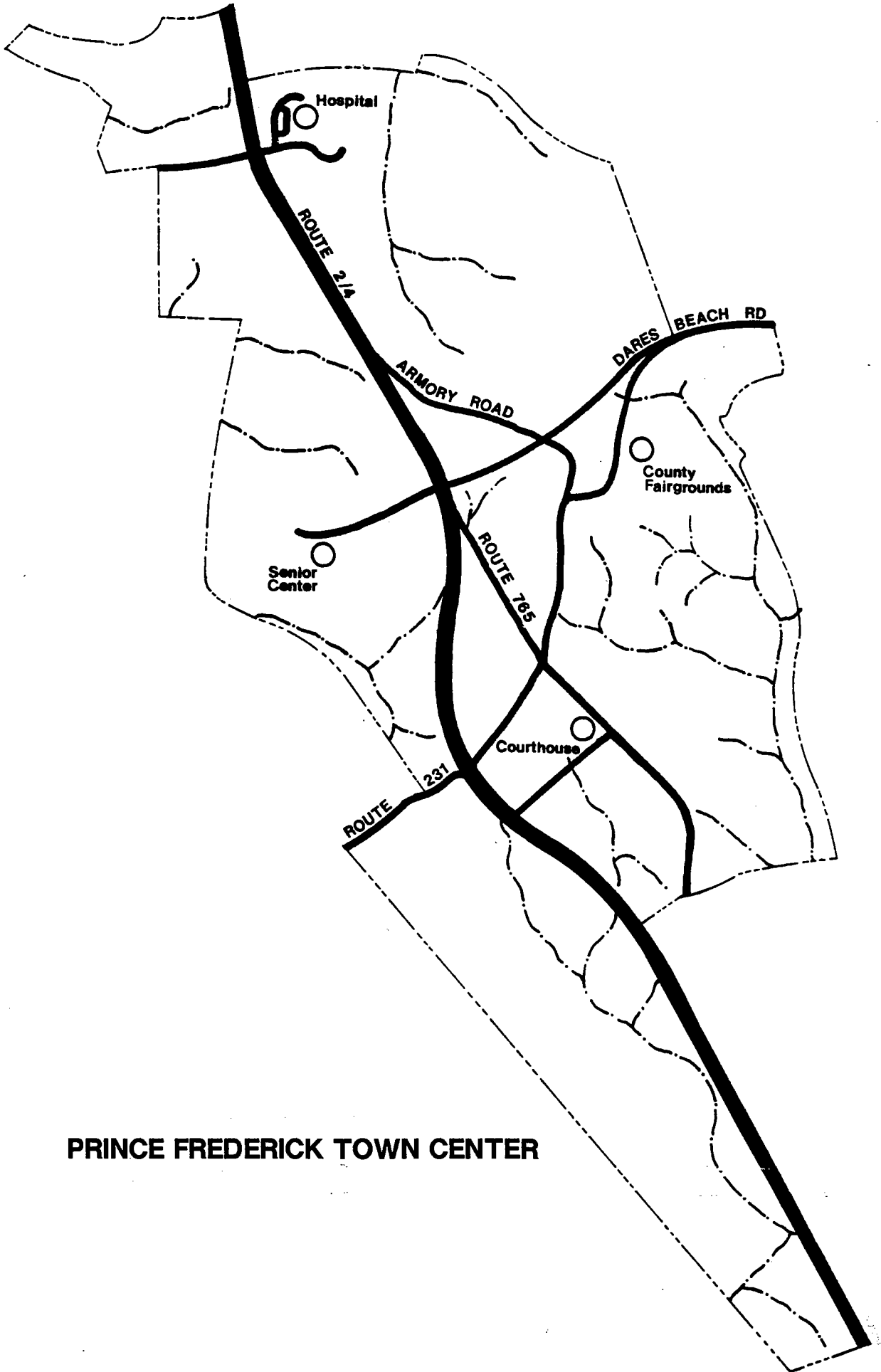
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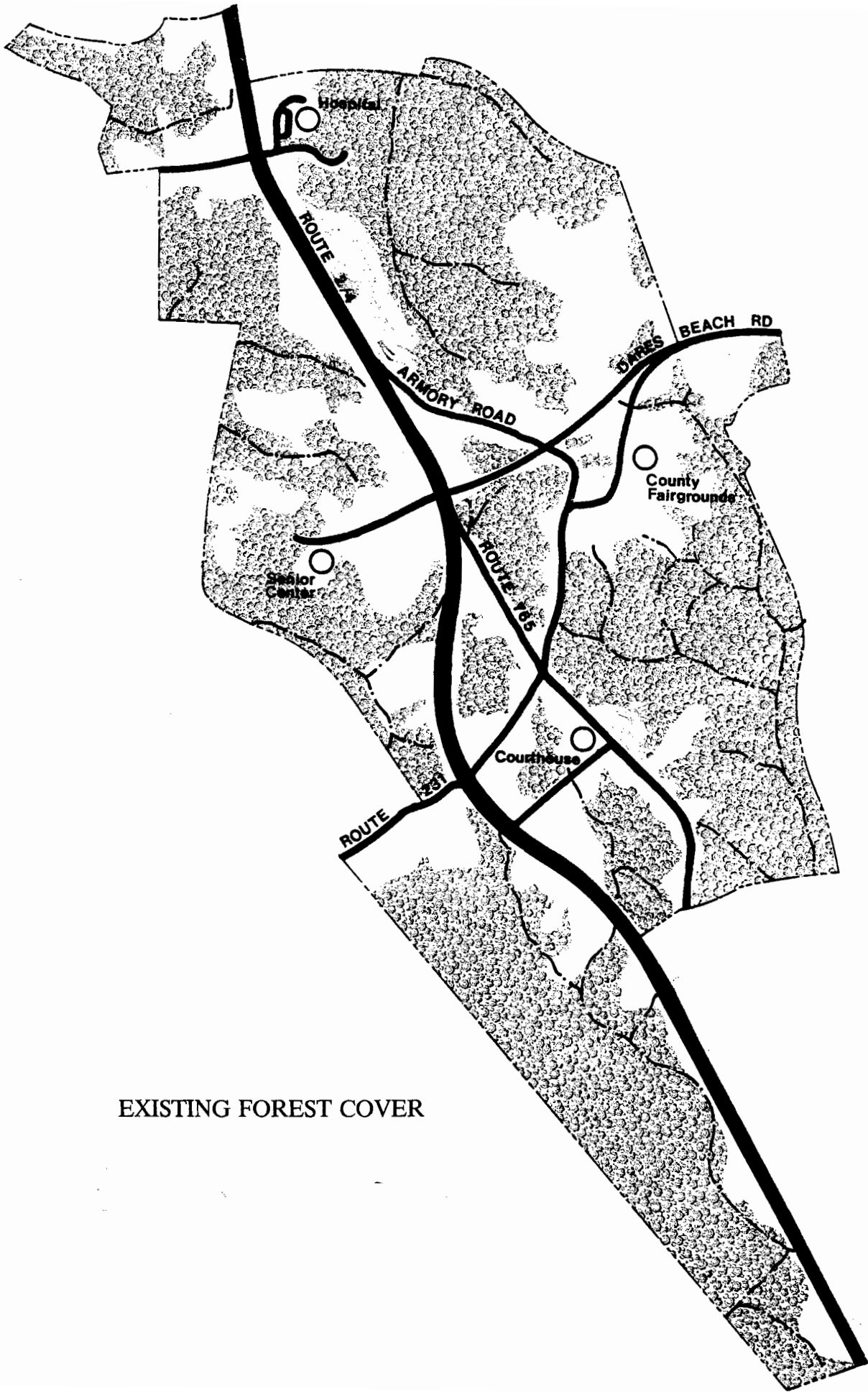
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**PRINCE FREDERICK TOWN CENTER**



EXISTING FOREST COVER

## INTRODUCTION

### PURPOSE

The Prince Frederick Zoning Ordinance is a legal document which is adopted to accomplish the aims of the Prince Frederick Master Plan by regulating land uses in a manner that promotes the health, safety and general welfare of Calvert County residents. The central aims of the Plan and the guidance for this Ordinance were identified and prioritized by the public as follows:

- A. ensure that public facilities (water, sewer and roads) are adequate to support growth,
- B. maintain high standards of road safety and minimize traffic congestion,
- C. preserve the historic heritage of Prince Frederick,
- D. promote pleasant, stable residential communities,
- E. preserve existing natural features (forests, large trees, stream valleys and natural areas), and
- F. promote economic development.

### EFFECTIVE DATE AND TERRITORIAL LIMITS

This Ordinance shall take effect immediately after adoption and shall apply to all the lands, uses and improvements within the territorial limits of the Prince Frederick Town Center as shown on officially adopted Calvert County Zoning Maps.

All site plans and subdivision plans which have received preliminary approval as of the above date shall be allowed to be constructed even if they do not conform to this Zoning Ordinance. However, if substantial construction is not completed within two years, approval is withdrawn and new plans which meet the requirements of this Zoning Ordinance must

be submitted and approved before work on the project can continue.

### APPLICABILITY

The Prince Frederick Zoning Ordinance is a supplement to the Calvert County Zoning Ordinance and applies to the Prince Frederick Town Center. Except in cases specifically addressed in the Prince Frederick Zoning Ordinance, the Calvert County Zoning Ordinance applies.

### SEPARABILITY

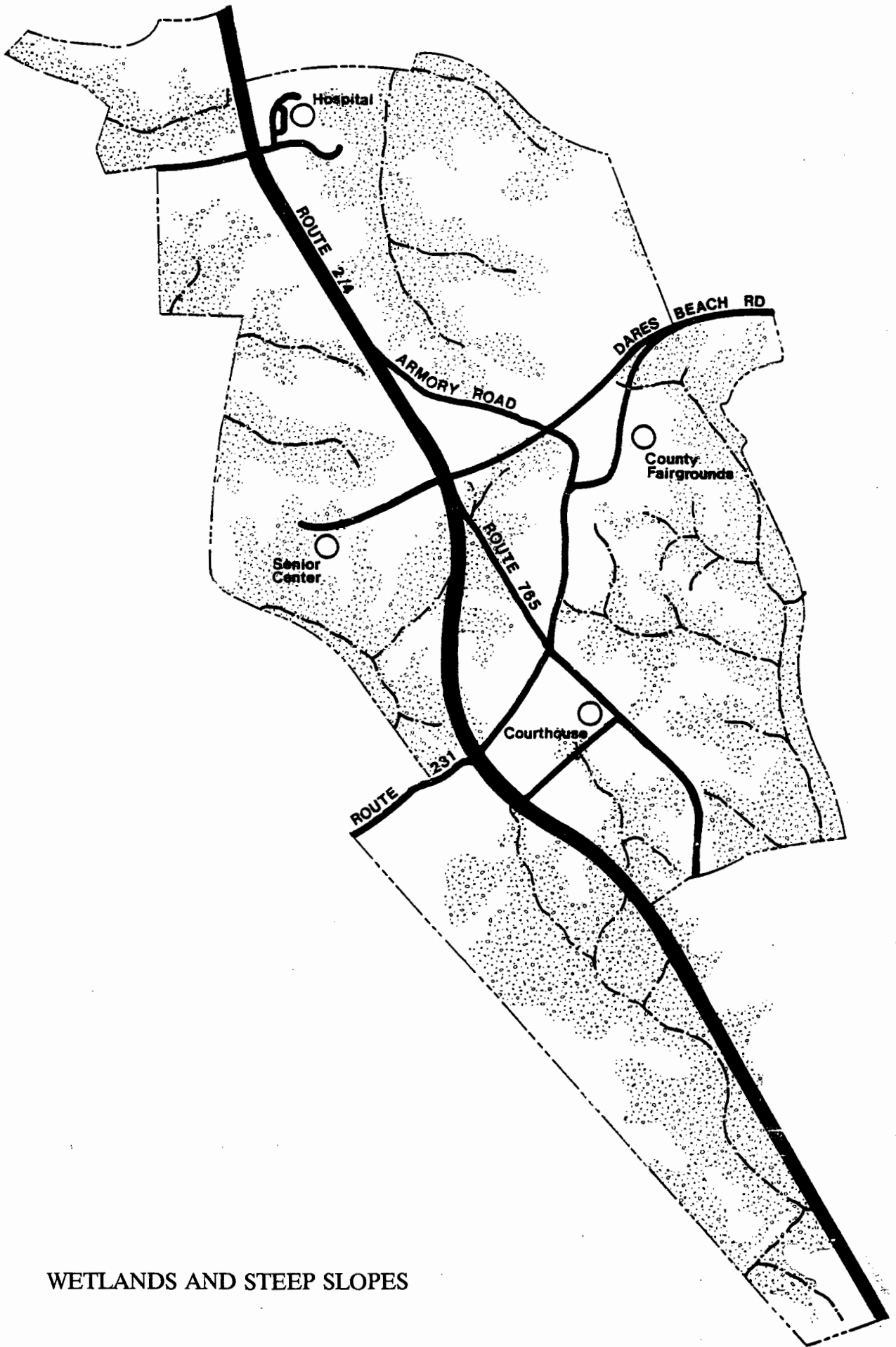
If any section, subsection, sentence, clause or phrase of this Zoning Ordinance is declared invalid or unconstitutional by a court of competent jurisdiction, such provision shall be deemed separate and such invalidity or unconstitutionality shall not affect the validity of the Zoning Ordinance in its entirety or of the remaining sections or parts thereof.

### INTERPRETATIONS

The Zoning Officer shall be responsible for clarifying the intent of this Zoning Ordinance. Should there be a question on the meaning of a section of the Zoning Ordinance, an "official interpretation" shall be approved and signed by the Zoning Officer.

### PERMITS

Building permits must be obtained before constructing, demolishing or erecting buildings or structures covering more than 150 square feet; signs; moving, adding to, or extending building or structures; or excavating for any construction.



WETLANDS AND STEEP SLOPES

I. NATURAL FEATURES  
 A. GRADING AND CLEARING

Grading and clearing permits may not be issued prior to site plan or subdivision approval.

....  
 Exceptions may be granted provided that:

1. Clearing is part of a state forestry management plan unrelated to preparation of the site for sale or development.

....  
 2. The issues related to the protection of natural features are deemed by the Planning Commission to have been satisfactorily addressed in a pending subdivision plat or site plan.

3. The area to be graded and/or cleared is less than 5,000 sq. ft. and will not disturb historical or archeological sites.

....  
 4. The County or State has determined that there is a need to grade or clear within the right of way (r.o.w.) for road safety purposes.

B. NATURAL VEGETATIVE AREAS (non-tidal Flood Plains, non-tidal Wetlands and USGS Blue Line Streams and their buffers)

(10/28/03) All of the provisions in Section 8-2 of the Calvert County Zoning Ordinance shall be met with the exception of the following (subject to federal and state approval):

1. Filling Wetlands

a. ...for Roads

Wetlands may be filled for the purpose of constructing approved County and State roads.

b. ...for Commercial Uses

Wetlands that are not within Flood Plains (as determined by the presence of alluvial soils or hydrologic studies) or within required Blue Line stream buffers may be filled for the purpose of providing space for commercial uses provided that it can be demonstrated that

the proposed square footage of the commercial project can not be accommodated on the site without filling the wetlands.

If wetlands are to be filled for the purpose of constructing a commercial use, a mitigation plan is required. Mitigation shall provide water quality benefits and plant and wildlife habitat equivalent to the wetland which is being destroyed or altered and shall be accomplished, to the extent possible, on-site or near the affected wetland and within the same sub-watershed.

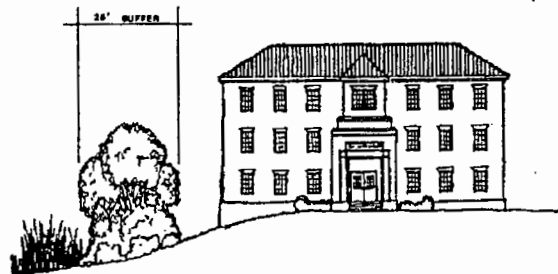
....  
 c. ...for Residential Uses

Filling of wetlands for the purpose of constructing residential dwellings is not permitted.

2. Reductions in Wetland Buffers

a. ...for Commercial and Public Uses

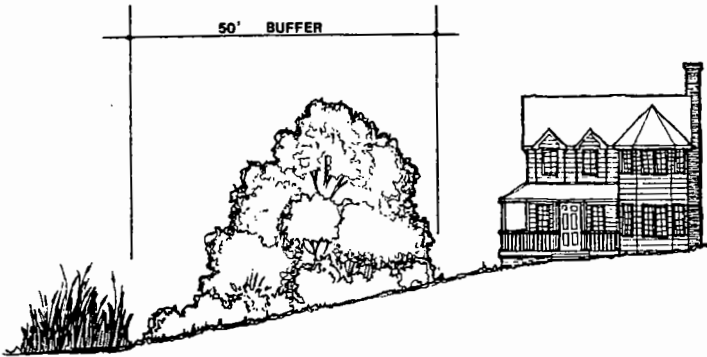
The required minimum 50 foot buffer adjacent To wetlands may be reduced to 25 feet for commercial and public uses if adequate methods are formulated by the developer and approved by the Zoning Officer, in accordance with the provisions in Section 8-2.05 of the Calvert County Zoning Ordinance and if the proposed square footage of the project can not be accommodated on the site without intruding into the 50 foot buffer. (10/28/03)



2a. The 50-foot wetland buffer may be reduced to 25 feet for commercial and public uses.

b. ...for Residential Uses

Reductions in the required 50 foot buffer are not permitted for residential dwellings.



2b. The wetland buffer may not be reduced for residential uses.

3. Development on Steep Slopes

Outside required Natural Vegetative Areas and their buffers, development on steep slopes is permitted provided adequate methods are formulated by the developer and approved by the Zoning Officer, upon recommendation of, but not limited to, the County Engineer, the County Environmental Planner and the Soil Conservation Service, to minimize soil run-off problems.

C. FORESTS AND MONUMENT TREES

1. Identifying Forests and Monument Trees

Forests and monument trees as defined below shall be identified on the preliminary subdivision plan or site plan. Trees with diameters of 12 inches or more at 4.5 ft. above ground and ornamental trees and shrubs shall be individually identified.

Forest: area containing at least 100 trees per acre with at least 50% of trees having a 2 inch diameter measured at 4.5 ft. above ground.

Monument trees: national, state or local champion trees; trees having a diameter measured at 4.5 ft. above the ground of at least 24 inches or trees having a diameter that is at least 75% of the diameter, measured at 4.5 ft. above the ground, of the current state champion of that species.

2. Minimum Forest Cover

On sites with existing forest cover, a minimum of 10% of a commercial or mixed use site and 15% of a residential site, including natural vegetative areas, shall remain forested. Care should be taken to retain and incorporate large trees (12" and more in diameter) into the site design.

Exception: In cases where grading and/or clearing remove more than these amounts of existing forest, the site shall be replanted to meet the minimum requirement. (Required landscaping will count toward fulfilling this requirement.)

Replanting shall consist of transplanted stock, relocated from the site or surrounding areas, that is greater than 1.5 inches diameter measured at 4.5 ft. above the ground or seedling stock and shall be bonded for one year.

3. Protection of Monument Trees

Monument trees located within all natural vegetative buffers and required setbacks shall be retained. Monument trees located outside natural vegetative buffers and setbacks shall be retained and incorporated into the site design wherever feasible.

D. CHANGES TO EXISTING TOPOGRAPHY

As a general guideline, changes to existing topography should be kept to a minimum. Buildings, parking lots and roads should be designed to fit the existing landscape with as few modifications as possible. Where changes to existing topography are necessary, the area should be graded in such a manner as to blend in with the surrounding natural topography.

## II. TRAVELWAYS

### A. ROADS

#### 1. Site Access

(10/28/03) All of the provisions in Section 6-4.01 of the Calvert  
(05/01/06) County Zoning Ordinance governing site access shall be met.

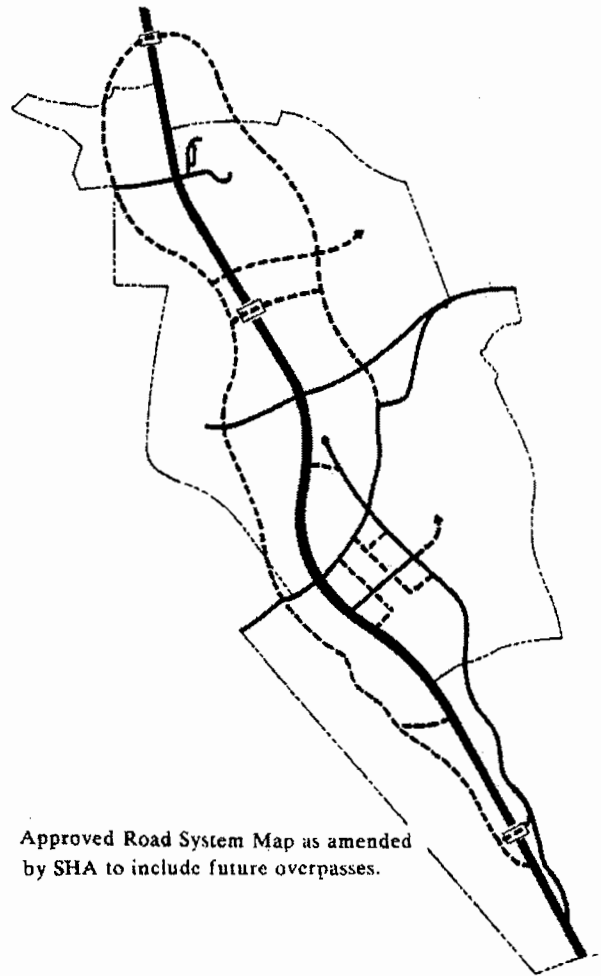
#### 2. Road System Map and Road Improvement Plan

New developments shall be consistent with the Road System Map and the Road Improvement Plan adopted in the Prince Frederick Master Plan.

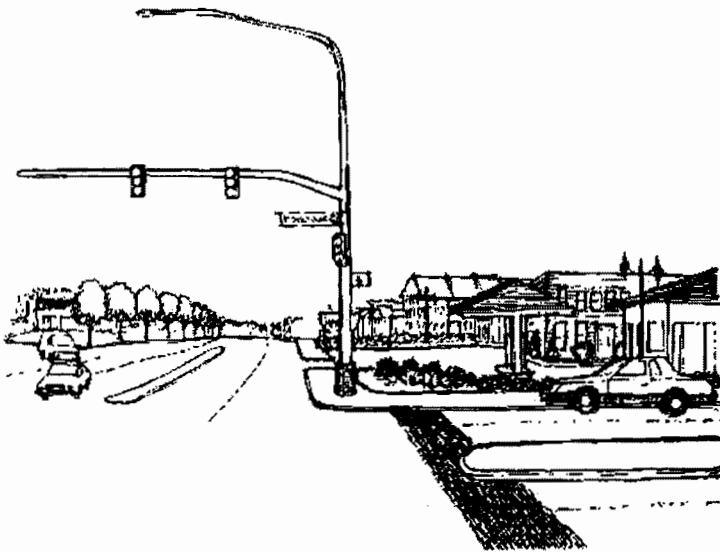
#### 3. Right-of-Way Width, Curb Radius, Cul-de-Sac Radius and Landscaping Within Right-of-Way

Exceptions to the Calvert County Road Ordinance may be approved by the County Engineer for the purpose of maintaining and/or creating a traditional neighborhood design. Exceptions may include reductions of the required right-of-way width, curb radius, cul-de-sac radius and landscaping within the right-of-way.

Approval will be based upon a) the number and type of uses to be served by the roadway b) adequate access for Fire and Rescue vehicles as determined by the Department of Public Safety and c) adequate provisions for maintenance of landscaping within right-of-way.



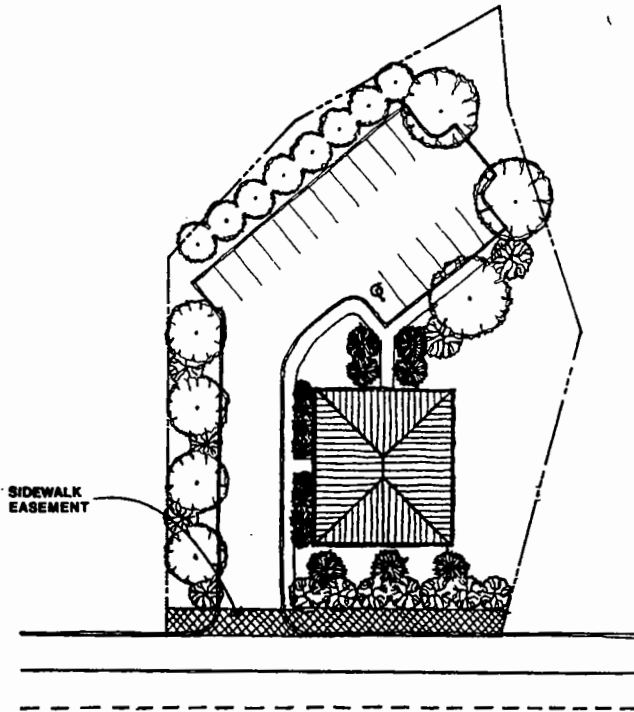
Approved Road System Map as amended by SHA to include future overpasses.



**B. SIDEWALKS**

**1. General Requirement for Sidewalks**

Sidewalks or sidewalk easements providing safe and convenient on-site circulation as well as access to adjacent sites and/or activity centers nearby are required. Sidewalks needed to serve a given site shall be constructed by the developer. Where possible, sidewalks shall be placed outside road rights-of-way.



**2. Sidewalk Maintenance**

Sidewalk maintenance shall be the responsibility of property owners, business associations, or homeowners' associations having frontage adjacent to sidewalks. A note shall be added to all new plats stating that the property owner will maintain sidewalks and plantings to curb line.

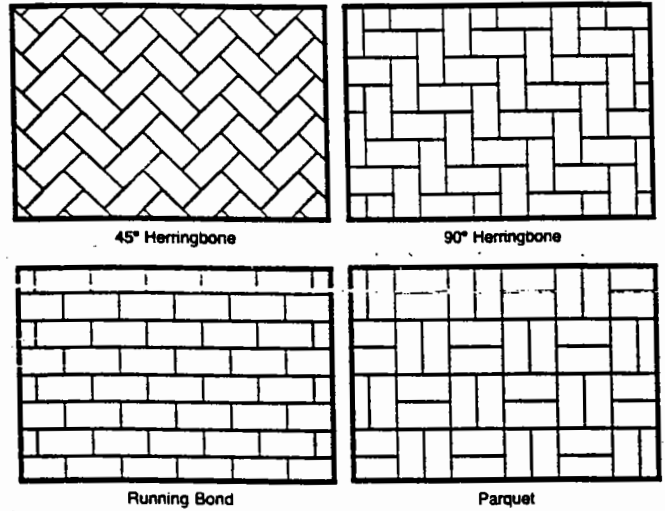
**3. Sidewalk Construction Standards**

Sidewalks shall be constructed in accordance with Department of Engineering specifications.

**4. Sidewalk Design Standards**

a. Red brick or brick appearance pavers in the designs shown below are the preferred sidewalk material for use in all areas of the Town Center. Other types, colors and designs of sidewalk material will need to be reviewed as part of Architectural Review and will be evaluated in accordance with the provisions listed in Chapter IV, Building Design.

b. Brick or brick appearance pavers are required to be used for a minimum 30% of the sidewalk running along front elevations of commercial and office buildings within the entire Town Center and along all public sidewalks in the Old Town District. (See Chapter VI for description of Districts.)



**C. BALTIMORE & DRUM POINT RAILROAD TRAIL**

Portions of the former Baltimore & Drum Point railroad right-of-way shall be preserved for use as a future pedestrian/bicycle trail or trails. As parcels develop, trail easements shall be deeded to the County as part of a Public Works Agreement.

### III. SITE DESIGN

#### A. PROMOTING A COHESIVE TOWN CENTER DESIGN

Prior to preliminary design of any project of more than 5,000 sq. ft of disturbed area (1000 sq. ft within the Old Town Residential District and Fairgrounds District\*), the applicant shall offer to meet with adjacent property owners, County staff and representatives of business and community organizations to discuss building and site design alternatives.

The objective is to look for alternatives that will

1. help establish a cohesive, logical, and easily understandable pattern of development and a "sense of place" within the Town Center by coordinating the placement and design of buildings, signs and landscaping,
2. use available space efficiently,
3. identify opportunities to establish amenities such as "vest pocket" parks and courtyards, shared outdoor seating areas and landmarks at minimal cost,
4. maximize the impact of advertising signs while contributing to the goal of creating an attractive appearance along roadways,
5. reduce site design costs through use of shared entrances, parking lots, stormwater management systems and the like,
6. minimize dependence upon automobiles by providing pedestrian access to convenience stores, recreational facilities and/or public transportation, where feasible, and
7. meet or exceed minimal standards and guidelines.

The purpose of the meeting is to generate ideas and provide assistance. Decisions are non-binding and can not overrule the provisions in this Ordinance.

\*See Chapter VI - Development Districts

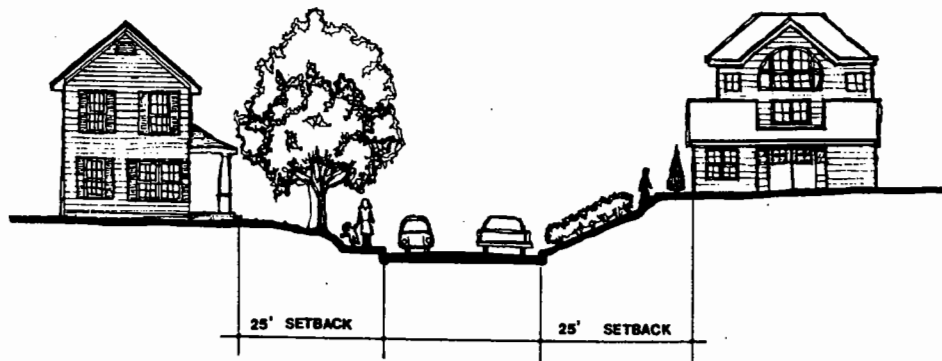
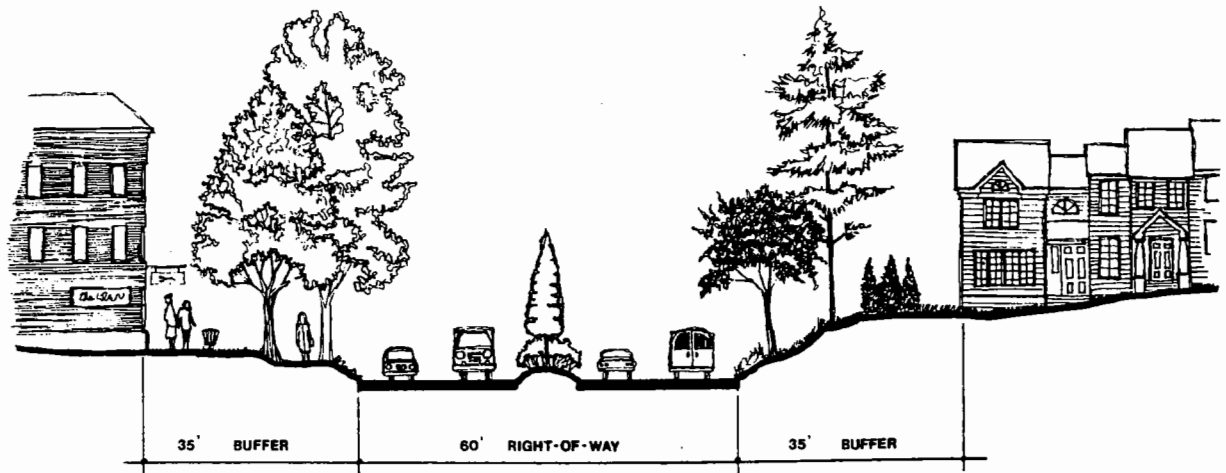
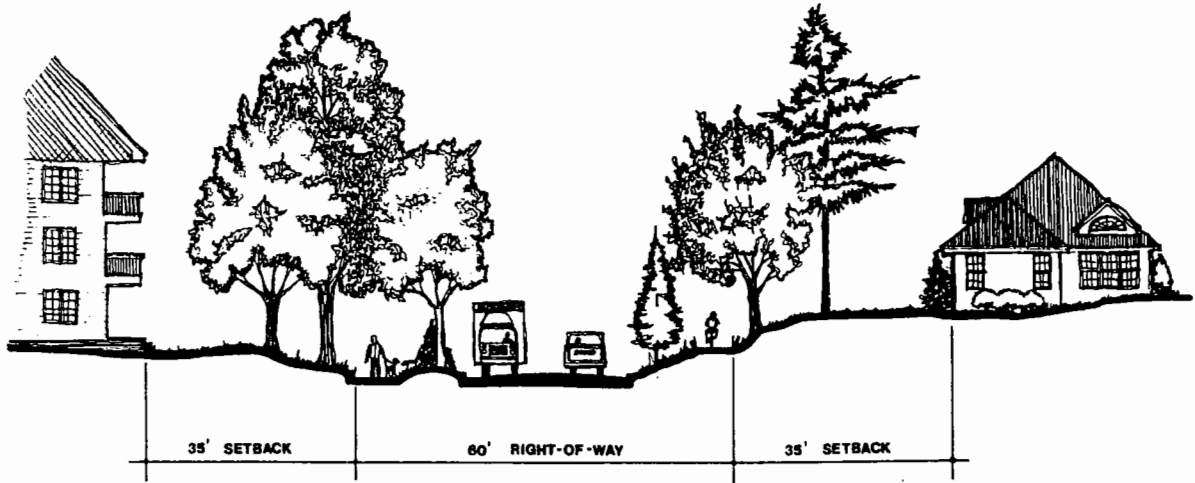
#### B. ARCHEOLOGICAL AND HISTORIC FEATURES

##### 1. Archeological Features

An inventory of existing on-site archeological features may be required prior to preliminary approval of a site plan, subdivision plat or grading permit. To avoid any potential delays, the applicant may make a written request for an inventory review prior to submittal of a grading and clearing or site plan application. Applicants are strongly urged to complete the inventory process prior to site design so that the preservation of significant features can be incorporated into the layout.

To determine whether an inventory of archeological features is required, an application for review will be reviewed by the Department of Planning & Zoning and submitted to the Historic District Commission, the Maryland Historical Trust and other relevant agencies to determine within 30 days of submittal whether any known significant archeological features are located on the property or whether the characteristics of the site suggest the probability of significant archeological features.

For the purposes of this Ordinance, a significant archeological feature is defined as a feature that has been identified by applicable County, State or Federal agencies as having unique archeological significance. In the event either of the above is found to be relevant, the applicant may be asked to conduct an inventory according to criteria established by the Department of Planning & Zoning upon recommendations by the relevant agencies or to preserve the artifacts in place.



**Setbacks**

## 2. HISTORIC FEATURES

In cases where a building which is listed on the Maryland Historical Trust Historic Sites Survey is located within the boundaries of a proposed development project, the applicant shall meet with the Historic District Commission prior to approval of a site plan or subdivision plat. The purpose of the meeting is to determine the feasibility of retaining the building and incorporating it into the site design. In the event it is not feasible to retain the building or make it available for removal to another site, the applicant will be required to document the structure prior to destruction in accordance with criteria established by the Department on Planning and Zoning.

Removal of an archeological or historic feature prior to approval of a site plan or subdivision plat may be grounds for denial of the application.

### C. SETBACKS AND LANDSCAPING WITHIN SETBACKS

1. Setbacks and Landscaping along Roadways
  - a. Building, Parking Lot and Accessory Use Setbacks along Roadways

The minimum setback for buildings, parking lots and accessory uses along all roadway rights-of-way within the Town Center is 35 feet.

## Exceptions

### i. Reductions in setbacks

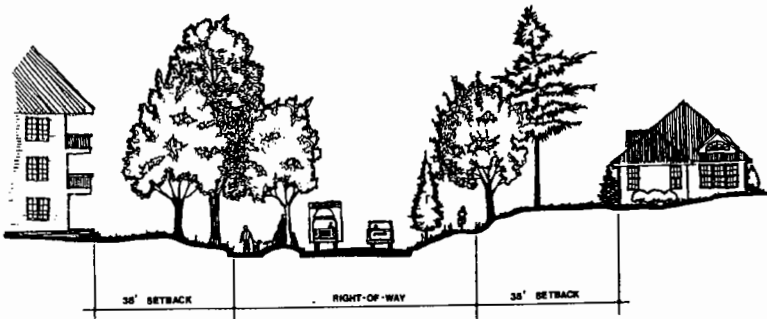
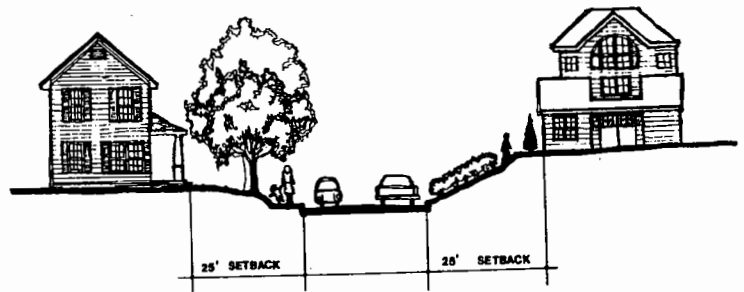
The following reductions in minimum setbacks may be approved by the Planning Commission to meet special design requirements of the Planning Commission or to address a non-self-imposed hardship.

(i). Setbacks may be reduced to a minimum of 25 feet from edge of right-of-way along proposed secondary "loop" roads.

(ii) Setbacks may be reduced to a minimum of 25 feet from edge of curb along all local connector roads (future roads that connect Rt 2/4 with secondary loop roads), Church Street, Armory Road, Fairgrounds Road, Dares Beach Road west, and Duke Street.

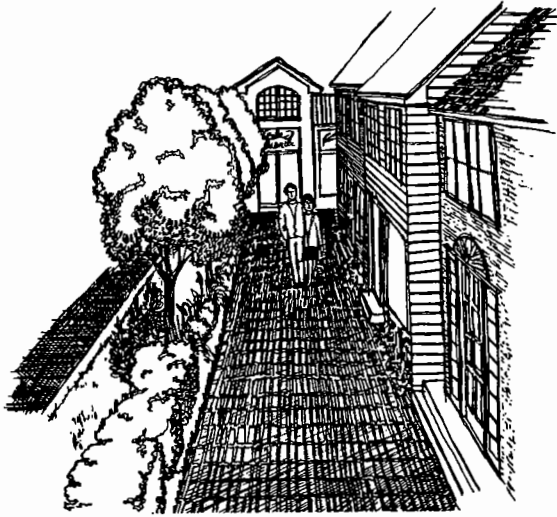
(iii) Setbacks may be reduced to a minimum of 15 feet from edge of curb along all local roads.

(iv) If an applicant is required to provide a service road, the Planning Commission may grant a reduction in the parking lot setback from 35 feet to no less than 20 feet.



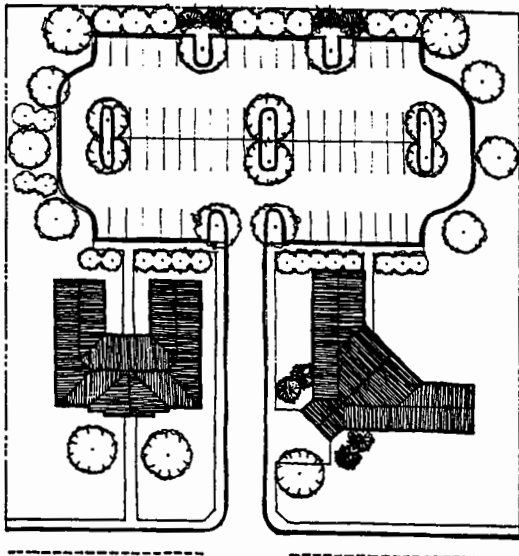
ii. Setbacks along Main Street.

Future buildings (including replacements of existing buildings) along Main Street shall maintain the existing as-built setbacks along the street, exclusive of public plazas and outdoor seating areas. Off-street parking lots shall be located to the rear of buildings.



iii. Setbacks along Proposed Loop Road west of Rt 2/4.

In lieu of parking lot setbacks, off street parking lots shall be located to the rear of buildings.



c. Landscaping within Setbacks along Roads

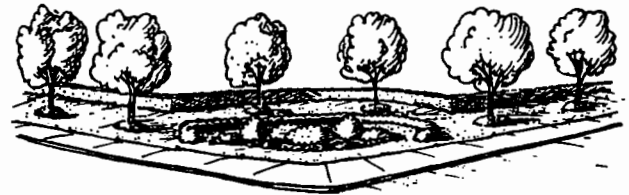
Landscaping within setbacks along roads shall include:

i. a single row of shade trees planted 40 feet on center

ii. a row of street lights posted every 60 feet or as specified by a lighting engineer, where needed for safety as determined by the Planning Commission.

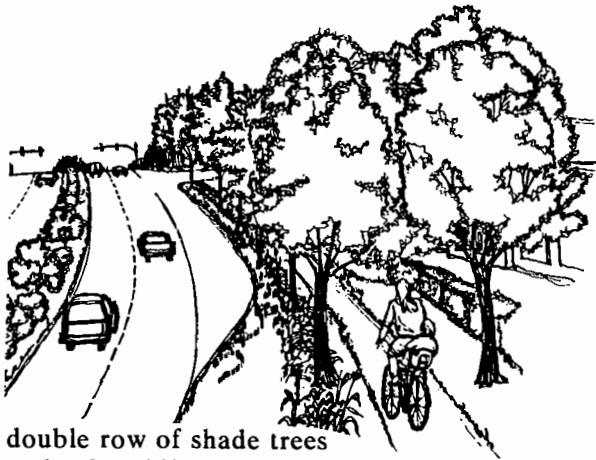
iii. a sidewalk with a minimum width of five feet

iv. at road intersections and driveway entrances, a minimum 100 sq. feet of groundcover and seasonal plantings at each corner.



## Exceptions

(i.) Along the west side of Rt 2/4, between the northern edge of the Town Center and the proposed new intersection across Rt 4 from Armory Road, a double row of shade trees planted 40 feet on center and an eight foot bikeway/sidewalk easement between tree rows is required.



double row of shade trees  
and 8 foot bikeway/sidewalk

(ii.) Along the west side of Rt 2/4, from Rt 231 to the southern edge of the Town Center, a forested edge is required. (A forested edge is defined as a no cut/no clear natural vegetative buffer.) In cases where clearing can not be avoided, the applicant shall replant in accordance with adopted reforestation procedures upon approval of the Planning Commission. Selective thinning is permitted to provide filtered views.



forested edge

(iii.) Along the proposed loop road west of Rt 2/4, an eight foot sidewalk/bikeway easement is required in lieu of the five foot sidewalk on the west side of the roadway; in the section located between Dares Beach Road and Rt 231, a forested edge, as described in c (ii.) above is required in lieu of shade trees planted 40 feet on center.



8 foot bikeway/sidewalk

(iv.) Along Armory Road south of Dares Beach Road, on both sides of the street, a forested edge is required in lieu of shade trees planted 40 feet on center.

(v.) Landscaping plans utilizing existing trees and shrubs are encouraged and may be substituted for any of the plans listed above provided they meet the intent of these provisions.

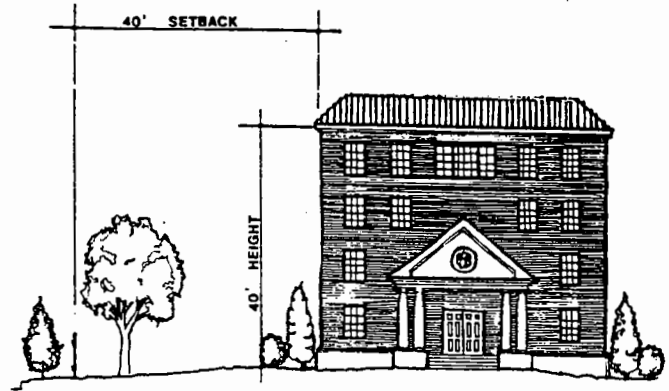
## 2. Setbacks from Existing Adjacent Parcels and Landscaping within Setbacks

### a. Setbacks from Existing Adjacent Parcels

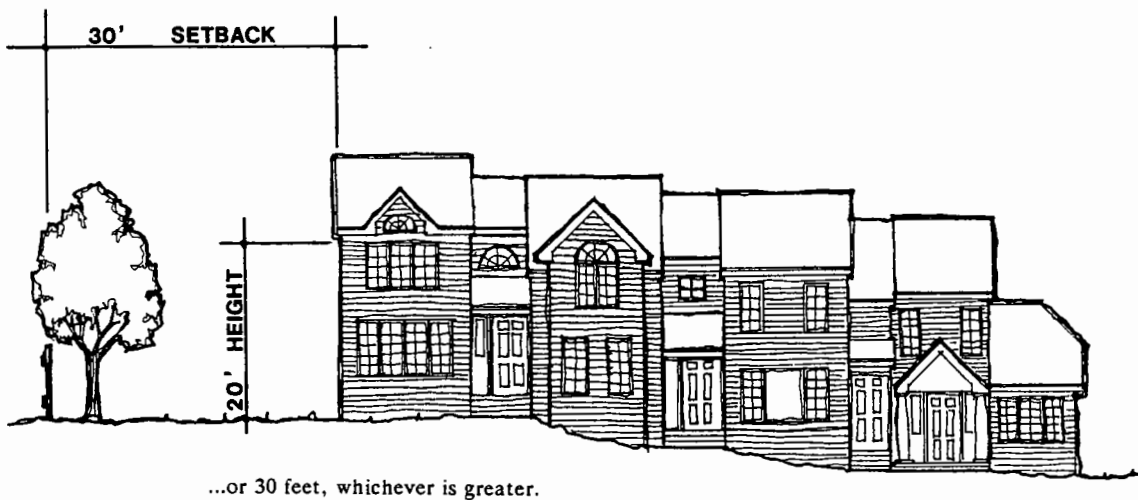
Setbacks from adjacent parcels shall equal the height of the building, exclusive of the roof, or 30 feet, whichever is greater. If variable roof heights are utilized, the average height of the building at the boundary to adjacent property shall be used to establish minimum setbacks.

#### Exceptions:

If a property is adjacent to a parcel that has an existing non-residential use, or if the intent is to continue an established pattern of residential development (ex. a row of townhouses), the setback may be reduced to 0' if emergency access and maintenance are properly addressed. If the adjacent use is residential, approval for a reduced setback must be obtained from the adjacent property owner prior to preliminary approval.



3a. Side and rear yard setbacks shall equal the height of the building, exclusive of the roof...



...or 30 feet, whichever is greater.

### b. Landscaping within Setbacks

Landscaping within side and rear yard setbacks shall be capable of creating a forest canopy with deciduous trees spaced a minimum of 40 feet apart in staggered rows or clustered together with scattered groupings of evergreens.

3. On-Site Building to Building Setbacks and Landscaping adjacent to Buildings

a. Performance standards in lieu of Setbacks

There are no minimum on-site building to building setbacks. However, for residential and mixed-use developments all of the following conditions shall be met:

- i. Units that are at ground level shall be provided with private outdoor spaces which shall be visually screened from each other and from public travelways (vehicular and pedestrian) to a height of at least 6 feet. (To qualify, screening must be permanent, year-round and require little to no maintenance). Exception: This provision is not required for single-family detached units on lots that are greater than 5,000 sq. ft.
- ii. Windows of individual units shall not directly face each other unless a minimum of 25 feet is provided between windows.
- iii. At least one wall of each unit shall be provided with windows looking onto a space at least 50 feet square (50' x 50').
- iv. Each unit shall be designed to ensure adequate light and air.
- v. No distinction shall be made, in terms of exterior siding and trim, between fronts, sides and rears of buildings.
- vi. Spacing between buildings shall be adequate to provide for emergency vehicle access as determined by the Department of Public Safety.

b. Landscaping Adjacent to Buildings

Landscaping adjacent to buildings is required to help reduce building mass, promote safe pedestrian circulation, accent buildings and draw attention away from parking lots, utility lines and outdoor storage areas.

D. MINIMUM LOT SIZE AND LOT WIDTH REQUIREMENTS

No minimum lot size or lot width is required. However, single family detached, single family attached and townhouse residences shall be provided with a private backyard at least equal in size to the footprint of the residence.

E. PERMITTED NUMBER OF DWELLING UNITS

(4/20/99)  
(12/02/03)  
(05/01/06)

1. Determining Number of Dwelling Units

The number of residential dwelling units that can be placed on any given site within the Town Center will be determined on the basis of regulations governing protection of natural features (Chapter I), road requirements (Chapter II), site Design standards (Chapter II) height requirements by District (Chapter IV) and Permitted Uses (Chapter VII). In no case may the number of dwelling units per gross acre exceed fourteen.

2. Requirements for use of Development Rights

Prince Frederick is designated as a Transfer Zone. Purchase of five Calvert County Transferable Development Rights is required for each dwelling unit over one (1.0) dwelling unit per acre<sup>1</sup>. A maximum of fourteen (14.0) dwelling units per acre is permitted.

Exceptions:

- a. In lieu of purchasing Transferable Development Rights, applicants with bona fide affordable housing projects may apply to the Board of County Commissioners for a waiver of the

<sup>1</sup> The use of Transferable Development Rights (TDRs) shall not be required for age-restricted housing developments that comply with Section 5-5 of the Calvert County Zoning Ordinance and which are properly submitted prior to July 1, 2006.

requirement to purchase Transferable Development Rights. The Board may approve the application in whole or in part provided that no more than 30% of the existing housing stock within the Town Center meets the State definition affordable housing.

## F. PARKING LOT LAYOUT AND DESIGN

### 1. On-Site Vehicular Flow

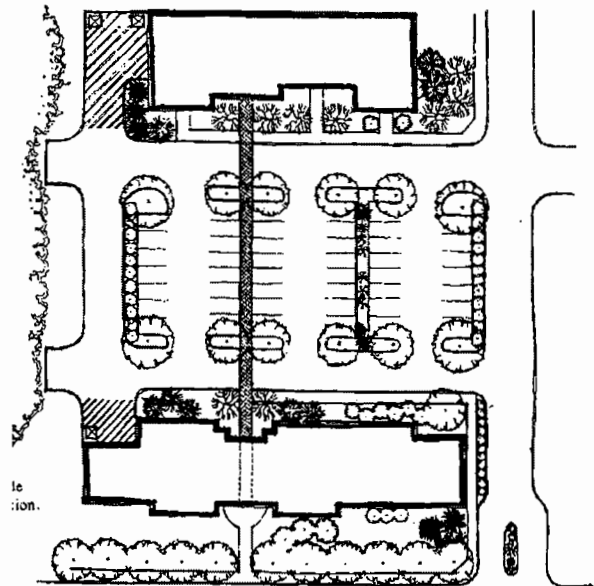
- a. Parking Lots shall be designed to prevent awkward or dangerous vehicular flow.
- b. Driveways in parking lots must be a min. 25 feet wide for two-way traffic and 18 feet wide for one-way traffic.
- c. Room must be available to allow vehicles to back out of parking spaces.
- d. A driveway must be set back at least 6 feet from a building corner or other visual obstructions to reduce blind spots.
- e. Driveways shall be located and designed to avoid conflicts between pedestrians and vehicles.
- f. Loading and unloading spaces for delivery trucks shall not block major pedestrian ways or create blind spots when trucks are loading and unloading.
- g. Large outdoor trash receptacles, such as dumpster boxes, shall be adequately screened and located so as to provide easy truck access and not conflict with parking or through traffic. Trash receptacles may not be located in residential parking spaces and must be on concrete or asphalt pads large enough to accommodate trash pick-up trucks.

### 2. On-Site Pedestrian/Bicycle Flow

- a. At least one handicapped parking space shall be provided for every 25 spaces or part thereof and shall be located to provide safe and convenient access to buildings. All of the

provisions required under State standards (ANSI 117.7) shall be met.

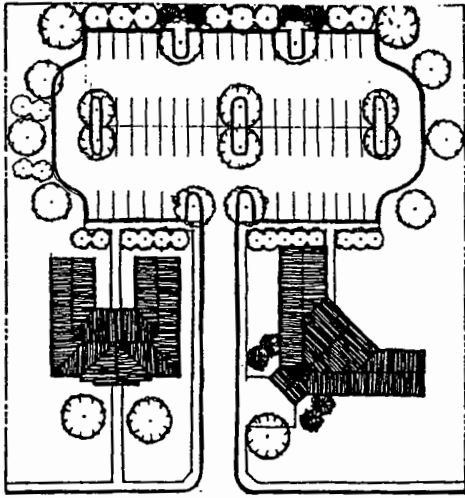
- b. Pedestrian walkways within parking lots shall be provided. Techniques such as landscaping and variations in pavement shall be used to help ensure safe pedestrian access from cars to buildings.
- c. The parking lot shall be designed to discourage pedestrians and vehicles from sharing the same pathways and to provide bicycle access. (10/28/03)
- d. Bike racks with space for a minimum of 10 bicycles are required for every 100,000 sq. ft. of retail space, every 50,000 sq. ft. of office space and every 50 units of multifamily.



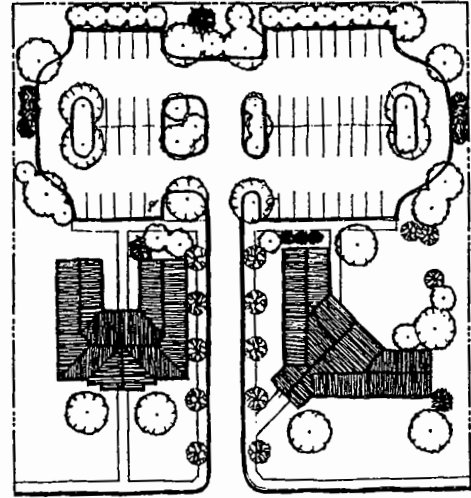
Parking lots can be designed to provide good vehicular and pedestrian circulation.

### 3. Large parking lot design

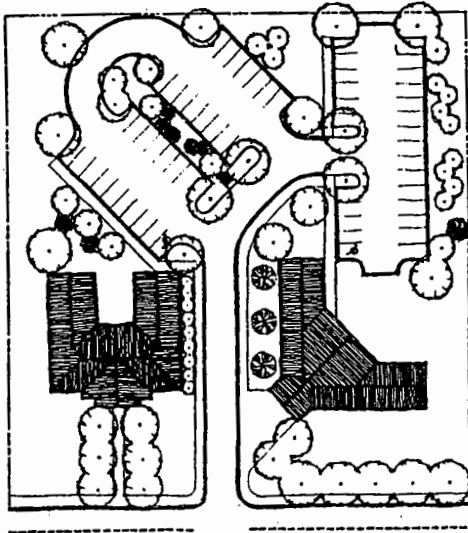
Large parking lots (more than 200 spaces) shall be designed to avoid creating the appearance of a large expanse of pavement adjacent to public roads and neighboring properties. The following techniques accomplish this goal:



a. Place parking lot to the rear of the building.



c. Use landscaping to visually break up the parking lot into several smaller lots



b. Break up the parking lot into several smaller lots

#### 4. Parking Space and Loading Requirements

##### a. Non-Residential Parking Space and Loading Requirements

(10/28/03) The parking and loading requirements in Section 6-3 of the Calvert County Zoning Ordinance shall be met.

##### b. Residential Parking Space and Vehicular Storage Requirements

- i. Each one-bedroom unit requires a minimum of one parking space. Two parking spaces are required for units with two or more bedrooms.

(07/03/01) Exception: for age-restricted multi-family housing the parking requirements in Section 5-1.12 of the Calvert County Zoning Ordinance shall be met.

(05/18/04)

(05/01/06)

- ii. An additional 15% of the total number of spaces provided for residents within a building cluster shall be provided for guests and overflow parking. Individual garages will not be counted as parking spaces.

- iii. A storage area for boats, recreational vehicles, trailers of all kinds, unlicensed vehicles and inoperative vehicles shall be provided. The area shall be a minimum of 200 square feet for every 5 units and shall be screened from the road and adjoining properties in accordance with the provisions for high screening listed in Chapter III, Section G.

Exception. If covenants prohibit these types of vehicles, this requirement may be waived.

#### 5. Landscaping within parking lots

Landscaping within parking lots is required for all parking lots having more than 25 spaces. The equivalent of one full sized parking space for every 12 spaces (or portion thereof) in excess of 25 spaces shall be landscaped with trees. Planting which is required for screening along the perimeter of a parking lot shall not be considered as part of the interior landscaping requirements.

Parking islands and ends of parking rows are to be landscaped with ground cover and at least one shade tree per island. A variety of shrubs and flowers are recommended in addition to the above.

## G. SCREENING

### 1. High Screening for Loading Areas, Outdoor Storage Areas, Trash Disposal Areas and others

High screening (minimum 6 ft.) is to be used where the objective is to completely conceal a use from general view. High screening is required to conceal loading areas, outdoor storage areas, trash disposal sites, and any other uses which are visually incompatible with neighboring land uses.

Materials used for high screening may be:

a. Evergreens (where the minimum height is 5 ft. at time of installation and where the plantings are capable of creating a continuous screen within 5 years. Evergreens may be planted in a close single row, staggered or offset rows or in clumps or groupings or

b. Solid/opaque wooden or masonry fencing. Where masonry or wooden fencing is utilized, a minimum two foot strip outside the fencing shall be landscaped with clusters of trees and shrubs with a maximum spacing between clusters of 25 ft.

c. Natural slopes and existing features which effectively conceal the use from general view.

d. A combination of the above.

Chain link fences are unacceptable.

Berms may be utilized in conjunction with any of the above.

### 2. Low Screening for Parking Lot Perimeters, accessory uses and certain outdoor spaces

Low screening (minimum 3 ft.) is required along the perimeter of parking lots in cases where the parking lots are visible from roads or adjacent (off-site) properties, to conceal accessory uses that are 4 ft. or less in height and to "enclose" outdoor spaces (such as pre-school play areas and playgrounds). Low screening shall not restrict sight distance needed for vehicular safety.

Materials used for low screening may be:

a. Evergreens (where the minimum height at time of planting is 12 inches and has the potential of reaching a height of at least three ft. within five years and where the plantings are capable of creating a continuous screen within 3 years.

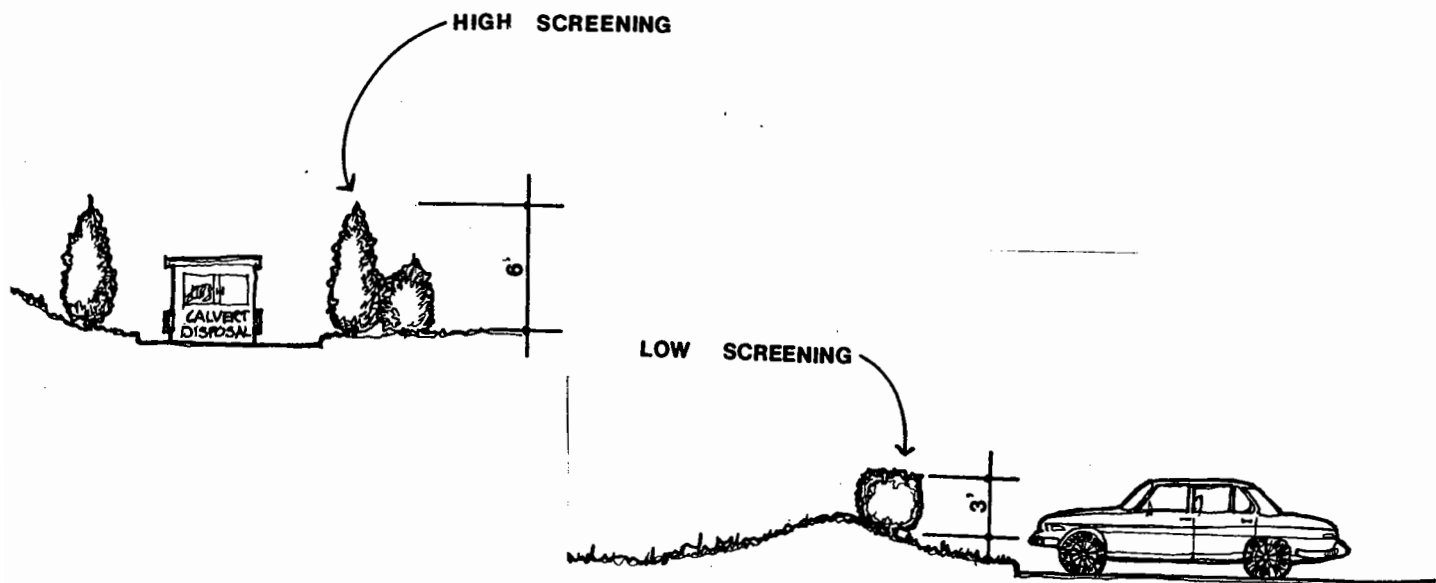
b. Solid/opaque fencing with a minimum height of 3 ft. in materials approved by the Planning Commission.

c. Natural slopes and existing features where they effectively conceal the use from view

d. A combination of the above.

Chain link fences are unacceptable.

Berms may be used in conjunction with any of the above.



## H. INSTALLATION AND MAINTENANCE OF LANDSCAPING MATERIALS

1. All plant material installed shall be undamaged and of the best quality.
2. All plant material installed shall be balled and burlapped or container grown. Bare-root plant material is acceptable, provided it is installed by a licensed nurseryman and guaranteed.
3. All trees shall be a minimum 1 1/2 inch caliper at installation.
4. A maintenance agreement for the plant material shall be included in the property covenants.
5. Plant material shall be bonded for one year. A planting schedule shall be included in the bond and shall be based on seasonal considerations.
6. Where possible, trees shall be planted outside the right-of-way.

## I. EXCEPTION TO LANDSCAPING REQUIREMENTS

1. Any of these landscaping requirements may be modified or waived by the Planning Commission in order to facilitate and promote the retention and use of existing vegetation.
2. Landscaping shall not be placed around structures in a manner so as to impair or impede accessibility for firefighting or rescue operations.
3. If proposed tree plantings conflict with preexisting underground utility lines or overhead wires, the applicant shall proposed an alterantive landscaping plan to be approved by the Planning Commission.

## J. OUTDOOR LIGHTING

### 1. Within Commercial Parking Lots and Walkways

Outdoor lighting is required on any parking lot of a shopping center, on any parking lot of a business having regular evening hours and along walkways. The lighting plan shall be prepared by an engineer registered in the State of Maryland.

### b. Within Residential Developments

Outdoor lighting is required on any parking lot, along pedestrian walkways and between parking lots and residences. No pole type security lighting will be permitted in residential developments except at the entrance to a development if it intersects an existing public arterial or collector road.

### c. Along Roadways

See provisions for lighting along roadways in Chapter III Section C.1c.

### d. Lighting Design

The design of outdoor lighting fixtures is subject to architectural review. Below is the preapproved lighting standard for use along pedestrian walkways within the Town Center. Applicants may propose alternative designs which will be evaluated on the basis of compatibility with the provisions in Chapter IV, Building Design.



Pre-approved lighting standard

## K. OPEN SPACE & RECREATION REQUIREMENTS

These requirements apply to all residential development projects with five or more dwelling units. (Use the Recreation Area Worksheet to determine how the following requirements apply to a given site).

1. During construction of the base road, recreation areas shall be graded and stabilized.
2. Basic minimum requirements: .05 acres (2180 sq. ft) of useable common outdoor open space per unit is required. No more than 25% of the total outdoor open space may be non-tidal wetlands.

Exception: The requirement for common outdoor open space may be waived for single family detached units having a minimum lot size of 5000 sq. ft. provided that all of the provisions for active recreation (below) are met.

Out of the total open space acreage required, a minimum of 200 square feet per unit shall be developed for active recreation and distributed as listed below.

- a. Playing fields/Village Greens - 150 square feet per unit (except as provided below)

Purpose: To provide a central focal point and outdoor gathering space for the community; to provide for informal play close to home for children ages 7-14. May also serve as net games area, "village greens", community house lawns and general open space. If more than one field is required, the Planning Commission may approve a swimming pool, tennis court or other type of active recreation facility within the boundaries of one of the fields.

### Minimum standards:

1-5 units: exempt

6-25 units: 5,000 square feet; 75'x75'

26+ units: 150 square feet per unit but in no case less than 10,000 square feet

size per field: 10,000 square feet

dimensions: 100'x100'

Must be level, open ground, good drainage; must be visible from the residential units the playing field is to serve, and must be accessible to children without crossing arterials or collector roads; must not be adjacent to public roads unless physical barriers adequate to prevent children from running out into streets are provided. These playing fields shall be graded at the time of base road construction.

Exceptions: Variations in sizes and dimensions of playing fields may be approved provided they meet the purpose and minimum standards listed above. In cases where more than 2 fields are required, up to 30% of the required fields may be located on moderately hilly or wooded terrain. (Heavily wooded, steep ravines will not qualify.)



**RECREATION AREA WORKSHEET**

**I. 1-5 dwelling units: N\A**

**II. 6-25 dwelling units:**

**A. Open Space Required: # of units \_\_\_\_\_ x .05 acres = \_\_\_\_\_ acres**

**B. Min. Active Recreation Required:**

1. Playing field (75'x 75'): 5000 sf

2. Mini-park 400 sf

3. Paved Area N\A

**TOTAL 5400 sf**

**III. 26 + dwelling units:**

**A. Total Open Space Required: # of units \_\_\_\_\_ x .05 = \_\_\_\_\_ acres**

**B. Total Minimum Active Recreation Area Required:**

# of units \_\_\_\_\_ x 200 sf = \_\_\_\_\_ sf

**Total Active Recreation Area to be distributed as follows:**

**1. Playing fields (10,000 sf. per field)**

To calculate # of playing fields required:

a. # of units \_\_\_\_\_ x 150 sf = \_\_\_\_\_ sf

b. Subtotal above \_\_\_\_\_ ÷ 10,000 sf = \_\_\_\_\_ (minimum 1)

c. Required # of fields \_\_\_\_\_ (whole # in dividend)

d. Total square footage in playing fields: \_\_\_\_\_ sf

**2. Mini-parks**

To calculate: # of units \_\_\_\_\_ x 15 sf = \_\_\_\_\_ sf

**3. Paved Area (50' x 42' half court; 50' x 84' whole court)**

To calculate:

a. # of units \_\_\_\_\_ x 21 sf = \_\_\_\_\_ sf

b. Subtotal above \_\_\_\_\_ ÷ 2100 sf = \_\_\_\_\_

(If less than 1, leave space blank)

c. Required # of paved areas \_\_\_\_\_ (whole # in dividend)

d. Total square footage in paved area \_\_\_\_\_ sf

**4. Remainder**

a. Total Active recreation required (line III.B) \_\_\_\_\_ sf

b. Total from line 1d. \_\_\_\_\_ sf

Total from line 2. \_\_\_\_\_ sf

Total from line 3d. \_\_\_\_\_ sf

Total \_\_\_\_\_ sf

c. Total Remainder (subtract 4b. from 4a. \_\_\_\_\_ sf

**Remainder may be used to provide a variety of recreational facilities including but not limited to: tennis courts, fitness trails, garden plots, playgrounds. Approval required.**

b. Miniparks (seating areas/preschool play areas) - Minimum 15 sq. ft. per unit.

**Purpose:** To provide informal outdoor seating areas close to home and at scattered locations throughout the community; to provide safe, enclosed outdoor spaces for preschoolers to play under adult supervision.

**Minimum Standards:**

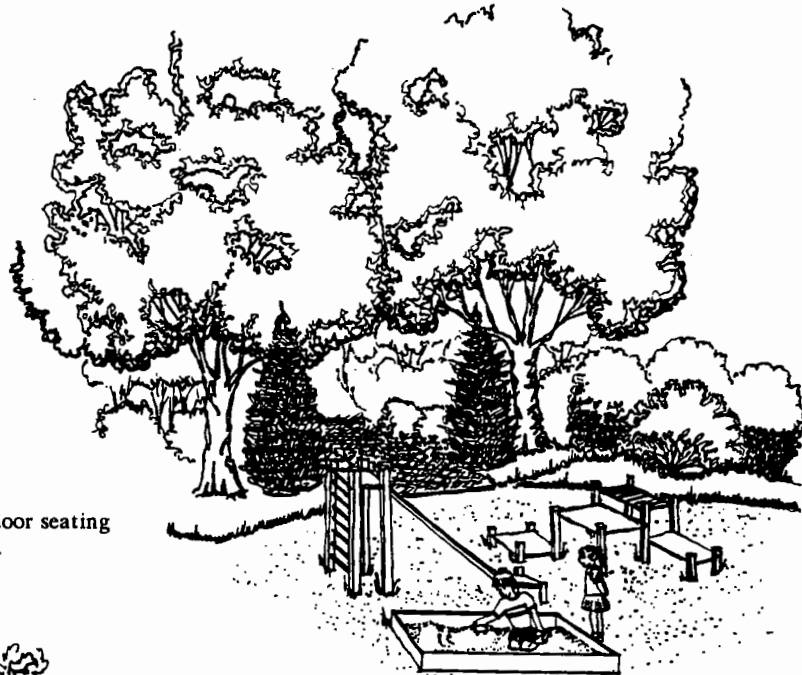
1-5 units: exempt

15 sq. ft. per unit but in no case less than 400 sq. ft.

A wide variety of sizes and designs of miniparks may be approved provided they meet the purposes outlined above. The essential elements are: (a) permanent seating for 3 to 6 persons, (b) landscaping to provide shade and amenities and to define and enclose the boundaries of the space and (c) safe locations.

Miniparks may be as small as 10'x12' and should generally be no larger than 40'x40'. The majority of the minimum required square footage is to be provided directly adjacent to unit clusters. Building offsets may be used to create small miniparks (i.e., courtyards, squares). At least one minipark should be located adjacent to waterfront, tennis courts, community house or other centralized recreation areas in order to enable adults to "keep an eye on the children" while watching or participating in sports activities and/or to provide adults with opportunities for passive recreation in a social setting.

A mini-park can be an outdoor seating area or a small play ground.



c. Paved Area - 21 sq. ft. per unit

Purpose: To provide facilities and space for basketball practice, handball practice, shuffleboard, roller skating, outdoor dances, formal net games. All ages, primarily 10 years to adult.

Minimum Standards:

Half court 50' x 42'

Whole court 50' x 84'

Construction and materials specifications to be approved by the Division of Parks and Recreation.

Must be visible from residential units and/or public areas.

Must have good drainage.

Equipment: Half court - 1 basketball backboard and net

Post holes for net games

Whole court - 2 basketball backboards & nets

Post holes or net games

N/S orientation is strongly encouraged.

Seating: Half court - permanent seating for a minimum of 6 persons

Whole court - permanent seating for a minimum of 9 persons

When more than 3 paved areas are required, 1 shall be lighted. Paved areas shall be graded at the time of base road construction.

Exceptions: Variations in size and dimensions may be approved provided they meet the purpose and requirements listed in (a) and (b) above.

d. Other.

The remaining required acreage may be utilized to provide a variety of recreational facilities including but not limited to: outdoor theatre/concert areas, gazebos, racquet courts, tennis courts, fitness trails, garden plots, playgrounds, handball courts. Approval will be based on appropriateness to the age groups and population to be served, location, and construction and maintenance standards.

3. Fees-in-Lieu

- a. If the requirement for 2,180 sq. ft of common open space per unit results in an open space dedication of more than 15% of the total site and

if all of the provisions for active recreation area and facilities listed in section K 2 have been met, the applicant may opt to pay fees-in-lieu of the additional open space acreage at an initial rate of \$1250.00 per unit. 1

This fee may be changed by the Board of County Commissioners to reflect changing land costs without amending this Ordinance.

L. INDOOR COMMUNITY SPACE

Residential developments having 100 or more units shall provide a minimum of 10 sq. ft of indoor community space per household.

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1. The amount of recreation area required is based upon the standard of providing 10 acres of community recreational open space per 1000 population and 5 acres of neighborhood recreational open space per thousand population. In 1991, this translates into the following dollar amounts:

Community Open Space Standard: 10 acres ÷ 1000 pop. = .01 acre per person x 2.8 persons per household = .028 acres per household x \$4,500 per acre (outside Town Center) = \$126.00 (rounded to 130.00).

Neighborhood Open Space Standard: 5 acres ÷ 1000 pop. = .005 acres per person x 2.8 persons per household = .014 acres per household x 80,000 per acre (inside Town Center) = \$1120.00

\$130.00 + \$1120.00 = \$1250.00

## M. COMMON AREA CONSTRUCTION AND MAINTENANCE REQUIREMENTS

1. In cases where common area and/or facilities are required, all required common area facilities must be completed by the time 70% of the total number of units have been issued use and occupancy permits. If a project is developed in sections, all open space requirements for the number of units in each section must be completed by the time 70% of the total number of units in each section have been issued use and occupancy permits. The number of units in a section may be no more than the number of units allocated for sewerage each year.
2. Prior to the transfer of title of any individual units or rental of any units, the applicant must submit verification that covenants are incorporated and/or a maintenance plan is in effect and that they contain the following provisions:
  - a. All common areas and facilities and all residential unit areas are defined in the legal description and are consistent with the approved site plan. Common areas are not to be deeded to an association until they are fully completed.
  - b. All owners within the development have a legal obligation to share the responsibilities for expenses associated with the management and maintenance of common areas and facilities.
  - c. All owners are granted perpetual easements or rights to the use of all common areas and facilities and all open space restrictions and reservations are permanent.
  - d. Owners are responsible for common areas and facilities as well as local taxes where applicable. The County will not be liable for common areas and facilities.
  - e. A system for assessing all units, including those still owned by the developer, and collecting dues sufficient to maintain the

common facilities is provided, where applicable.

- f. A statement that the Calvert County government has the right, under extreme circumstances, where the owners are unable to perform their responsibilities to protect health and safety, to maintain and operate the common areas or require a court-appointed trustee to administer the affairs of the owners, to assess the homeowners for the cost of this service and to provide a vehicle by which the homeowners can reassume management.

In addition, a statement must be provided and signed by the developer stating that all purchasers of units are to be fully informed of the existence of covenants, the extent of the common areas and facilities and the responsibilities of each homeowner to participate in the costs and maintenance of the common areas and facilities.

## N. APPLICATIONS FOR APPROVAL

1. If a residential development project will involve both site plan and subdivision approval, both applications must be submitted and reviewed at the same time.
2. In cases where residential uses are located within non-residential buildings, residential parking space, recreational open space requirements and setback requirements may be reduced by the Planning Commission.



#### IV. BUILDING DESIGN AND HEIGHT REQUIREMENTS

A. Buildings and accessory structures shall be designed to reflect Prince Frederick's 270 year history as the county seat by using traditional design elements that are typical of the Southern Maryland/Mid-Atlantic region. Within this basic design framework, buildings and accessory structures may vary from detailed copies of traditional architectural designs to contemporary interpretations of these designs.

Exception: Buildings within the immediate vicinity of the hospital should continue the established contemporary style.

B. Exterior siding for all sides of buildings and accessory structures shall be limited to brick, wood, wood appearance (vinyl, aluminum) horizontal or vertical board, shingles, stucco, split face block and stone. Uncovered cinder block and metal siding (other than metal clapboard) are not permitted.

C. Colors and tones on walls and roofs shall be muted. Bright and/or shiny colors, if used at all, shall be used as accents only.

D All mechanical equipment shall be screened from view.

E. All of the design elements on the building shall be kept in scale with the building.

F. All of the design elements on the building shall be in the same architectural style as the overall style of the building.



G. All of the design elements that are associated with a building (fences, storage sheds, etc.) shall be in the same architectural style as the overall style of the building.

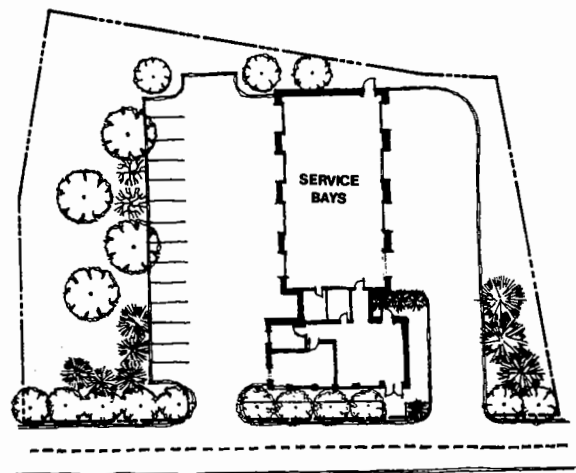
H. Large disparities between the height, width and length of a building shall be avoided. Building mass shall be broken up into smaller components.

I. Blank (windowless) walls at ground level adjacent to major pedestrian travelways are not permitted. Blank walls at ground level which are not adjacent to major pedestrian travelways must be screened.

J. Pitched roofs shall be a minimum 6:12 pitch. Flat roofs shall be modified through the use of pediments and other traditional design motifs. False mansard roofs are not permitted.

K. Buildings and accessory structures shall be compatible with neighboring buildings and structures in terms of height, proportion, rhythm and scale.

L. Service bay openings shall not be visible from public rights-of-way.



M. Building Height* Restriction By District (See Chapter VI for map of Districts)	
1.. Old Town District Old Town Residential District Fairgrounds District	Two stories plus a habitable roof. Maximum height 36 ft.
2.. Old Town Transition District Entry District Village District Forest District New Town District	40 feet plus a habitable roof except that areas where there is a water supply system which is capable of providing adequate supply for fire protection, is certified by the agency providing fire protection, and is in compliance with Article 38A, Annotated Code of Maryland, a height of 50 feet, plus the roof, shall be permitted. See exceptions in Section 5-1.11 (Residential) and 6-2.02 (Non-Residential) of the Calvert County Zoning Ordinance.

(10/28/03)  
(05/01/06)

\*Height is measured at street level front entrance except as required by the Department of Public Safety

GUIDELINES RELATING TO HEIGHT,  
PROPORTION, RHYTHM AND SCALE

**HEIGHT** - New buildings within the Town Center should conform with the average height of the adjacent buildings. A uniform skyline will project a harmonious atmosphere and visual continuity. (See height requirements on preceding page).

**CONSIDER** - Relating the overall height of new construction to that of adjacent structures. As a general rule, construct new buildings to a height roughly equal to the average height of existing buildings on and across the street.



**AVOID** - New construction that greatly varies in height from older buildings in the vicinity.

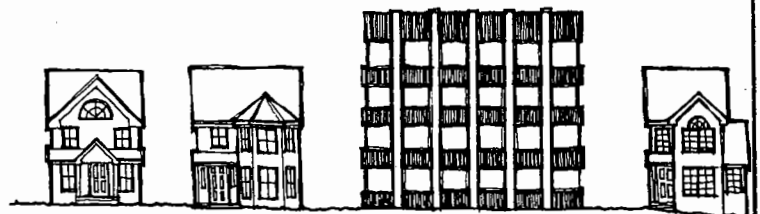


**PROPORTION** - The proportion, or the relationship between the width and height of the front elevation of the building, should be similar to the adjacent buildings. Proportion can also apply to the relationship of the windows and doors to each other and their relationship to the building itself.

**CONSIDER** - Relating the vertical, horizontal, or non-directional facade character of new buildings to the predominant directional expression of nearby buildings. Horizontal buildings can be made to relate to the more vertical adjacent structures by breaking the facade into smaller masses that conform to the primary expression of the streetscape.



**AVOID** - Strongly horizontal or vertical facade expressions unless compatible with the character of the structures in the immediate area. The new building shown does not relate well to either its neighbors or the rhythm of the streetscape because of its unbroken facade.



**RHYTHM** - The rhythm of the buildings and their components is the spacing or repetition of architectural elements or details with like elements. The regularity and frequency of elements such as doors, windows, or porches, and their placement within a facade, is a type of rhythm. Rhythm between buildings can exist when building types are repeated along a street. Regular and constant rhythms between buildings and components can help unify the streetscape.

**CONSIDER** - Respecting the recurrent alternation of wall areas with door and window elements in the facade. Also consider the width to height ratio of bays in the facade. The placement of openings with respect to the facade's overall composition and symmetry should be carefully studied.



**AVOID** - Introducing incompatible facade patterns that upset the rhythm of openings established in surrounding structures. Glass walls and window and door shapes and locations shown in the example are disrespectful to the adjoining buildings.



**SCALE** - Scale is the relationship between architecture and man or between the architectural mass and the space which surrounds it. The scale of the Town Center should be intimate in nature. Any buildings built on a monumental scale will seem out of place.

**CONSIDER** - Relating the size and proportions of new structures to the scale of adjacent buildings. Although much larger than its neighbors in terms of square footage, the building shown maintains the same scale and rhythm as the existing buildings.



**AVOID** - Buildings that in height, width, or massing violate the existing scale of the area. The new building shown disrupts the scale and rhythm of the streetscape, although it might be appropriate in a different location.



## V. SIGN DESIGN

- A. Illuminated signs shall be shielded so that no direct rays of light shall cause glare or reflection on any portion of an adjacent highway or residential building.
- B. No sign shall have moving parts or project any intermittent or flashing illumination except signs that indicate time and temperature.
- C. No sign shall be displayed which uses the words "Stop" or "Danger" or presents or implies the need or requirement of stopping or the existence of danger as part of an advertising sign.
- D. No sign shall be located which will interfere with traffic visibility on the right-of-way of any road or on any slope or drainage easement of the road.
- E. No sign shall be permitted which contains statements, words or pictures of an obscene character.
- F. No sign shall be placed on rocks, trees, or on poles maintained by public utilities.
- G. Portable signs are not permitted.
- H. No sign shall be permitted which becomes unsafe or endangers the safety of a building, premise, or person. The Zoning Officer shall order such signs to be made safe or repaired or removed, such order to be complied with within 15 days of the receipt of such order.
- I. When a sign structure does not include advertising information for a period of 120 continuous days, such sign structure shall be deemed a violation and shall be removed.

- J. Signs shall be removed when the business, enterprise, etc. advertised ceases to exist, or at the expiration date specified in the permit for erection; removal shall be the responsibility of the owner of the property on which the sign is located.
- K. Unless determined by the Zoning Officer to be safe at a lesser distance; the setback from the right-of-way shall be a minimum of ten feet.

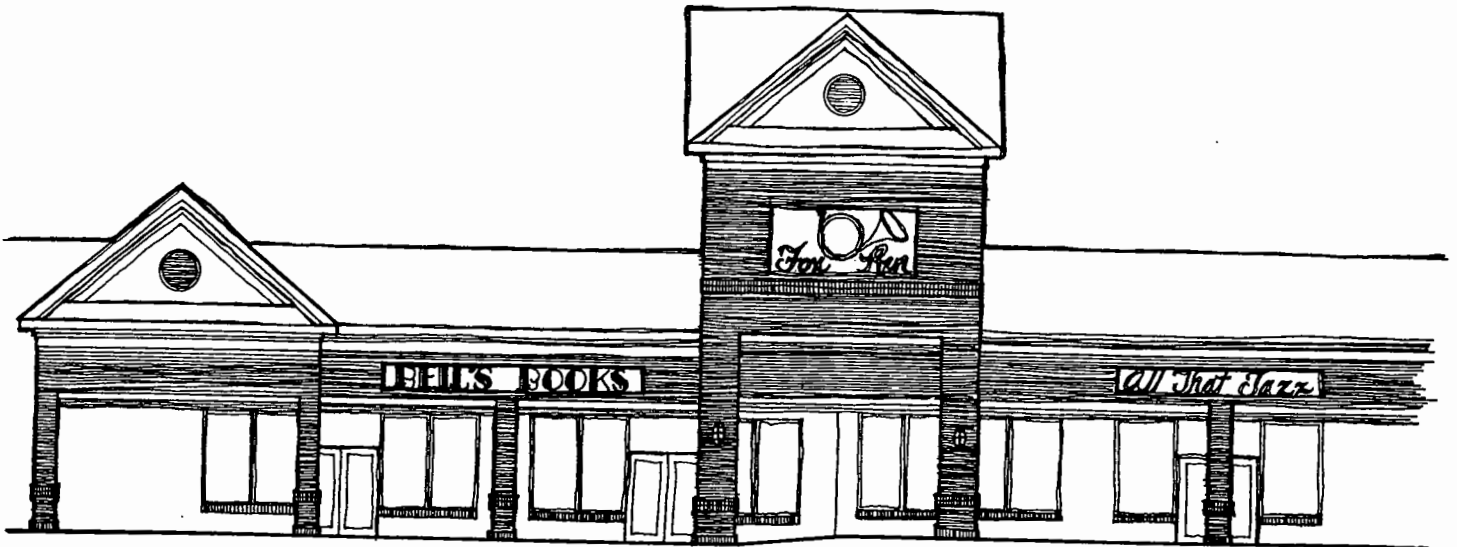
Exception: Any sign which is attached to the ground located on a minor arterial road and within 40 feet of the side lot line shall adhere to the front yard requirement for the district in which it is located. If located on a collector or local road and within 30 feet of the side lot line, the sign shall conform to the front yard requirement for the district in which it is located.

## L. Building Sign Design

1. Every building sign shall be designed as an integral architectural element of the building.
2. Signs on buildings shall not obstruct architectural features of the building. Signs shall not be mounted on roofs, extend above the roof line or above the second story of the building.
3. All signs for buildings within a shopping center, office complex, commercial park or business center shall be uniform in design. Uniformity is defined as
  - a) having the same (or similar) dimensions or proportions
  - b) having the same (or complementary) background colors (where applicable)
  - c) being of the same material.
4. The number of graphic elements (i.e., words and/or symbols) on a sign shall be held to the minimum needed to convey the sign's major message. A rule of thumb is to limit the number of syllables and symbols to 10.

## M. Freestanding Sign Design

1. Every freestanding sign shall be compatible with the building(s) to which it relates in terms of style, colors, proportion and scale.
2. All individual signs on a freestanding base shall be uniform in design. Uniformity is defined as
  - a) having the same (or similar) dimensions or proportions
  - b) having the same (or complimentary) background colors (where applicable)
  - c) being of the same material.
3. Reader boards shall be limited to two lines and shall have the same background color and width dimensions as the primary sign.



Example of uniform sign design within a shopping center.

(5/29/01) 4. Height for Freestanding Signs

- a) The preferred height for all freestanding signs located along Route 2/4 is 10 feet or less.
- b) The maximum height allowed along Route 2/4 is 12 feet.
- c) The maximum height allowed along Route 231, Dares Beach Road West, and the Prince Frederick loop road is 10 feet.
- d) Exceptions:  
Taller signs up to 14 feet may be approved provided that one or more of the following conditions exist:
  - i) A single sign represents more than one business.
  - ii) Site conditions are such that the only alternative is to place the sign below the grade of the existing adjacent roadway. In such a case, the height may be increased to equal the permitted height measured from the adjacent roadway grade.
  - iii) The location of the sign creates a non-self-imposed hardship. This may include obstructions within the line of sight or unusually long distances from a right-of-way.
  - iv) Along Route 2/4, an additional two feet of framing may be added if it adds an architectural element or for a company logo that enhances the overall appearance and effectiveness of the proposed sign (for a maximum height of 14 feet).
- e) Height of freestanding signs along other existing and future roadways is a maximum height of 8 feet.



Monument signs such as this are preferred.

5. The base of the sign shall be solid and designed as an architecturally integrated element of the sign.
6. The number of graphic elements (i.e., words and symbols) on a freestanding sign shall be held to the minimum needed to convey the sign's major message. A rule of thumb is to limit the number of syllables and symbols to 10.
7. One freestanding sign is permitted per site. The Planning Commission may, upon recommendation by the Prince Frederick Architectural Review Committee, permit additional freestanding signs in cases where the site is very large (more than 1000 feet of road frontage) provided that signs are architecturally integrated with each other and with the site. Freestanding signs shall not be located within parking lots.

N. Materials and lighting for Signs

1. Building signs and freestanding signs on sites having frontage along Route 2/4, Dares Beach Road West or Route 231 west of Route 2/4 may be wood, plastic or metal and backlit.
2. Building signs and freestanding signs on sites having frontage along all other existing and future roads shall be wood or wood appearance using indirect lighting only.

O. MAXIMUM SIGN AREA

The total square foot area of all signs shall be based on the following table.

TABLE I. TABLE OF MAXIMUM SIGN AREAS

Length of Front Building Wall (ft.)	Maximum Square Footage of Sign (sq. ft.)
10-19	30
20-29	50
30-39	70
40-49	90
50-59	110
60-69	125
70-79	140
80-89	155
90-99	170
100	SEE NOTE 3 BELOW

NOTES

1. Fractions will be rounded off to the closest integral number.
2. A cumulative measurement shall be taken for businesses sharing party walls.
3. For building walls which are longer than 100 feet, add one square foot of signage for every linear foot over 100 feet.
4. For each side of a commercial building, the signage shall be calculated for the length of that building wall and the signs shall be located only on that side of the building.
5. Calculation of sign area in square feet: Sign area shall normally be the area of the face of the sign, except for the following situation:

If the sign consists only of letters or figures, the area shall be calculated as if a rectangle were drawn around the exterior of the letters or figures.

## 2. Maximum Sign Area for Freestanding Signs

**TABLE II TABLE OF MAXIMUM SIGN AREA FOR FREESTANDING SIGNS**

a.	Along Rt 2/4	90 sq. ft
b.	Along Rt 231 and Dares Beach Road; all other existing and future roads within the Entry District, Village District, New Town District and Forest District	50 sq. ft
c.	Along Armory Road, Church Street Duke Street and Main Street; all other existing and future roads within the Old Town District & Old Town Transition District	20 sq. ft

**Exception:**

Signs within Old Town Residential District and Fairgrounds District are limited to 4 sq. ft.  
(See Chapter VI. for description of Districts)

## 3. Maximum Size of Building Signs

To determine the maximum size of signs on buildings, subtract the square footage used for all freestanding signs from the total permitted square footage shown in Table I.

## P. Replacement of Existing Signs

When an existing building sign or freestanding sign is replaced with a new sign, the new sign shall comply with the standards of this sign ordinance. A new sign is defined as any sign which advertises a new business.



## VI DEVELOPMENT DISTRICTS

### INTRODUCTION

#### 1. Development District Boundaries Established

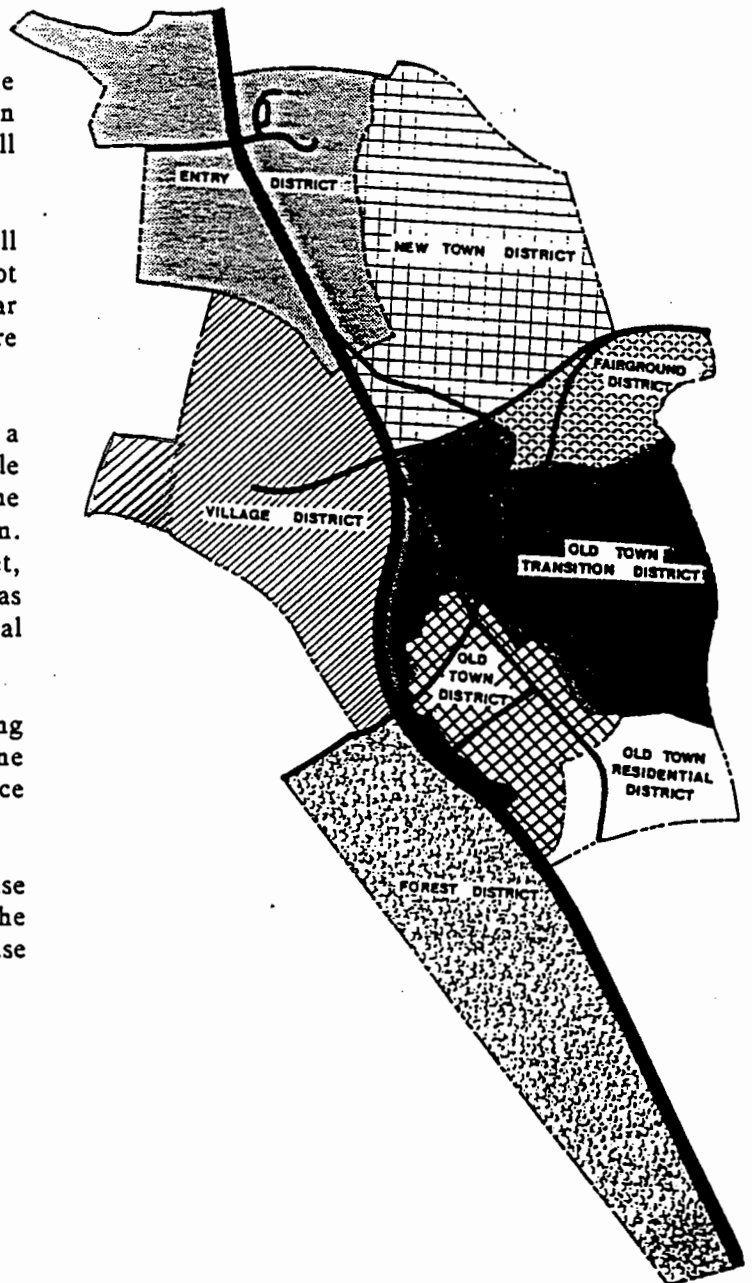
The locations and boundaries of the districts are established as shown on the maps entitled "Zoning Maps". The maps and all notations, dimensions, references and other data shown, as well as properly attested amendments, are incorporated as a part of this Ordinance. Maps reflecting the current status of zoning are located in the Department of Planning and Zoning.

#### 2. Interpretation of District Boundaries

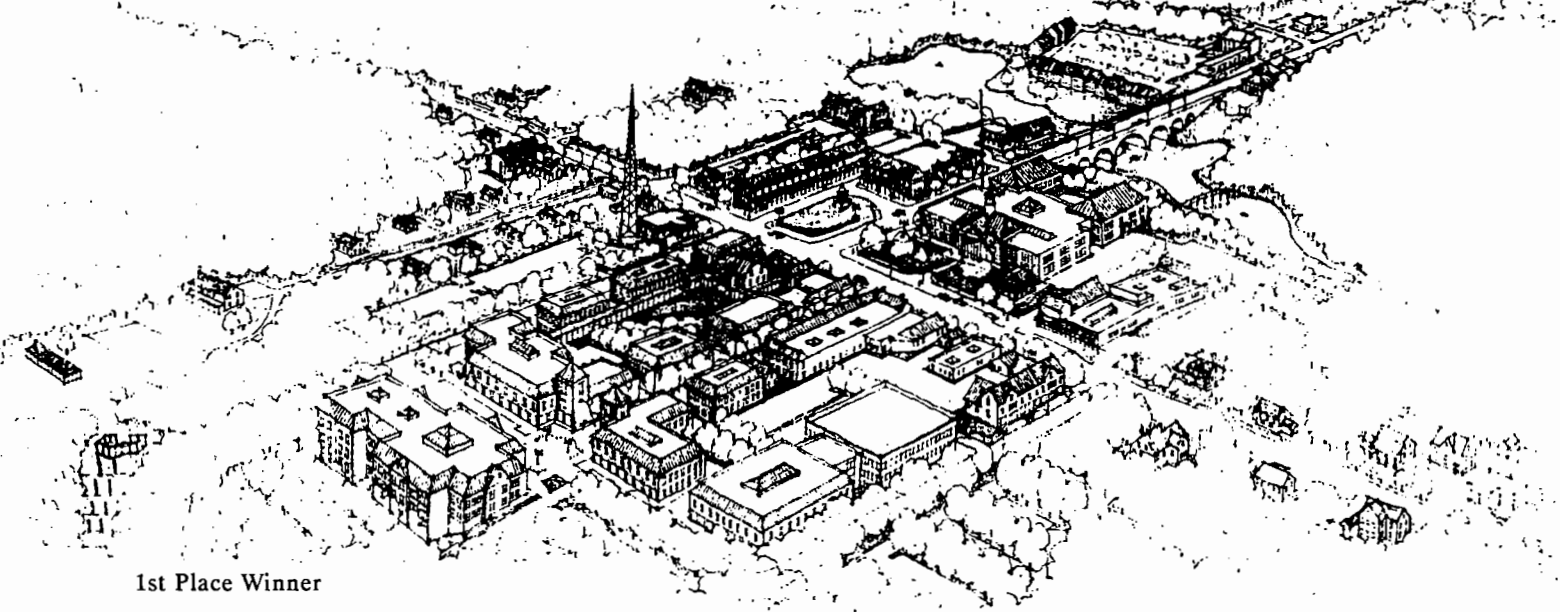
Where uncertainty exists with respect to the boundaries of the various districts as shown on the Zoning Map, the following conditions shall apply:

- a. District boundaries are meant to and shall be considered to follow street, alley or lot lines, or lines parallel or perpendicular thereto, unless such boundaries are otherwise shown on the Zoning Maps.
- b. Where a boundary line is shown within a street, alley or navigable or non-navigable stream, it shall be intended to follow the center line of such street, alley or stream. Should the actual location of such street, alley or stream vary from the location as shown on the Zoning Map, then the actual natural location shall control.
- c. Where a boundary line is shown as being located a specific distance from a street line or other physical feature, then this distance shall control.
- d. In unsubdivided property, unless otherwise indicated, a district boundary line on the Zoning Map shall be determined by the use of the scale on the Zoning Map.

- e. Wherever any road, alley or the public way is abandoned by official action as provided by law, the zoning districts adjoining the side of such public way shall be automatically extended, depending on the side or sides to which such lands revert, to include the right-of-way of the public way thus vacated, which shall thenceforth be subject to all regulations of the extended district(s).

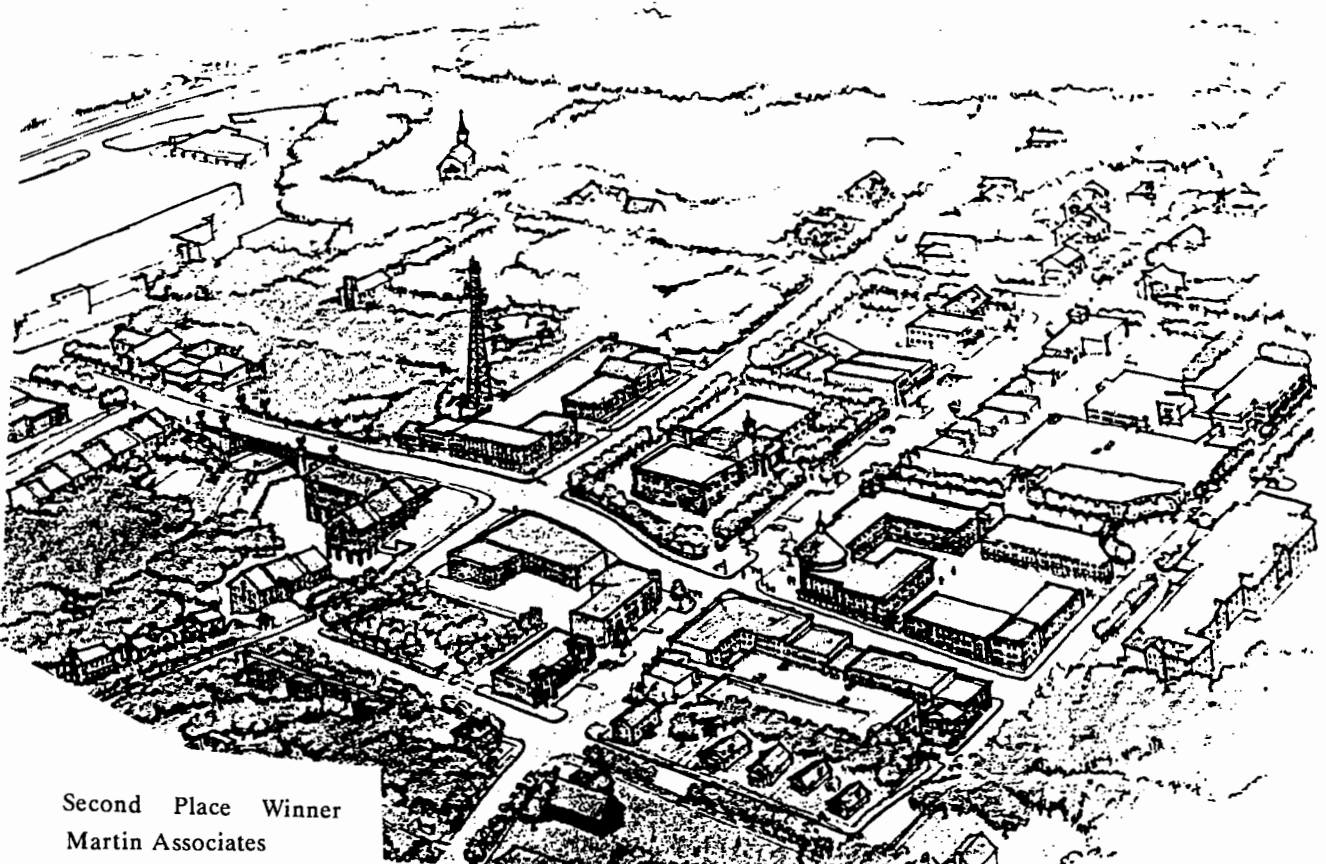


**1990 Prince Frederick  
Town Design Competition**



**1st Place Winner**

**Graham Landscape Architects and Muse-  
Wiedemann Architects**



**Second Place Winner**  
**Martin Associates**

## A. OLD TOWN DISTRICT

### 1. Existing Situation

The Old Town District is the heart of Prince Frederick. Its high concentration of government-related uses, its late 19th and early 20th century architecture and its old town pattern of closely spaced buildings and narrow, tree-lined streets are its dominant characteristics.

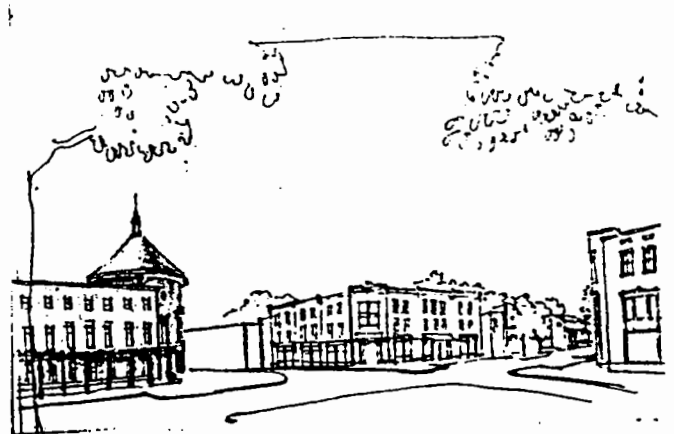
### 2. Guidelines for Future Development

This District is intended to remain the center of County government and will continue to be one of the primary employment centers in the County. It is a prime location for businesses that benefit from proximity to government agencies as well as businesses that can provide goods and services to office workers. The area has strong potential as a place for people to live who enjoy a comfortable, small-town environment. Substantial emphasis will be placed upon implementing the ideas and concepts that were identified in the 1990 Prince Frederick Design Competition.

### 3. Special Development Standards for the Old Town District

In addition to the general standards listed in this ordinance, the following provisions shall be met:

- a. Buildings shall be located in accordance with a traditional grid pattern of blocks and streets.
- b. Subject to Planning Commission approval, applicants may choose to pay fees-in-lieu of some or all required on-site parking spaces. The amount of the fee shall be based upon current parking lot land and construction costs as determined by the County Engineer and updated annually. Fees shall be used to construct public parking within the Old Town District.
- c. Buildings shall be designed to be compatible with the prevailing architectural style within the District and with reference to the adopted design concept for the Old Town District.
- d. Along Main Street, the ground floor level of non-residential buildings shall include display windows in order to help promote retail businesses.



## B. OLD TOWN RESIDENTIAL DISTRICT

### 1. Existing Situation

The Old Town Residential District is an integral part of the old town. Its prevailing character is that of an established community of large old houses along a quiet, tree-lined street. Most of the houses are white painted clapboard or red brick. The majority are late Victorian in style.

### 2. Guidelines for Future Development

This District is intended to maintain its existing old town residential character, to provide for single-family residential development as well as low intensity office and service uses that are compatible with the residential character of the area.

### 3. Special Development Standards for the Old Town Residential District

In addition to the general standards listed in this Ordinance, the following provisions shall be met.

a. Buildings shall be designed to be compatible with the prevailing architectural style within this District and with reference to the adopted design concept for the Old Town District.

b. Roof pitch: min 6:12 (Flat roofs are not permitted within this district).

c. Siding materials are restricted to a) horizontal wood or wood appearance siding, preferably painted white or cream or b) brick in red tones.



## C. OLD TOWN TRANSITION DISTRICT

### 1. Existing Situation

The Old Town Transition District currently contains a variety of uses within a comfortable, small town, neighborhood setting.

### 2. Guidelines for Future Development

This District is intended as a mixed-use (residential, commercial, office) area that will visually and functionally link the old and newer parts of town. A primary goal is to retain the existing small-town character of the area. Particular attention needs to be given to the problem of making new developments fit into the existing fabric of the community and to maintain and reinforce a sense of community.

Safe and convenient pedestrian and bicycle access to the Old Town District from neighborhoods within this District will be needed.

### 3. Special Development Standards for the Old Town Transition District

Siding and roof pitch: same as Old Town District

## D. FAIRGROUNDS DISTRICT

### 1. Existing Situation

The Fairgrounds District is a quiet residential area that together with portions of the Old Town Transition District provides a balanced mix of housing types within a comfortable, small town neighborhood setting. Architectural styles within the community are varied although they are all similar in terms of height, proportion, rhythm and scale.

### 2. Guidelines for Future Development

New development within the District will be primarily residential although certain commercial and office uses are permitted. The existing small town, residential character of this area shall be maintained. Particular attention needs to be given to the problem of making new developments fit into the existing fabric of the community and to maintain and reinforce a sense of community.

Safe and convenient pedestrian and bicycle access to the Old Town District from neighborhoods within this District will be needed.

In accordance with the Prince Frederick Transportation Plan, Fairgrounds Road will remain a residential street; through traffic is to be directed onto Armory Road. A section of Armory Road will be realigned.

### 3. Special Development Standards for the Fairgrounds District

#### a. Building Orientation and Streetscape

To help establish a sense of neighborhood with existing residences, buildings shall face existing buildings along the street and, where topography permits, be designed in accordance with a traditional grid pattern of blocks and streets.

#### b. Building Design

Buildings within this District are required to have a minimum roof pitch of 6:12. Flat roofs are not permitted.



## E. ENTRY DISTRICT

### 1. Existing Situation

The area within the boundaries of the Entry District currently contains a small scattering of commercial and residential uses and a considerable amount of land that is currently being farmed. A shopping center is being constructed on the east side of Rte 2/4 within this District.

### 2. Future Development Guidelines

The Entry District is designed to accommodate a wide variety of uses ranging from large shopping centers and office corporations to small, independently owned businesses.

In accordance with the Prince Frederick Transportation Plan, a new road will be built to the west of Rte 2/4 which will eventually link Stoakley Road to Rte 231. The purpose of this road is to divert some of the local traffic from Rte 2/4 in order to reduce traffic volume on the highway, minimize conflicts between slower moving local traffic and faster moving through-traffic, and to promote efficient utilization of Town Center land.

Landscaping, signage and architectural requirements for developments along Rte 2/4 are designed to establish a strong, uncluttered, clearly defined entryway into the center of Prince Frederick.

Landscaping, signage and architectural requirements for developments that will front along a proposed future roadway running parallel to Rte 2/4 on the west side of the highway are designed to combine the visual and functional qualities of a typical, old town "Main Street" with the modern convenience of ample parking.

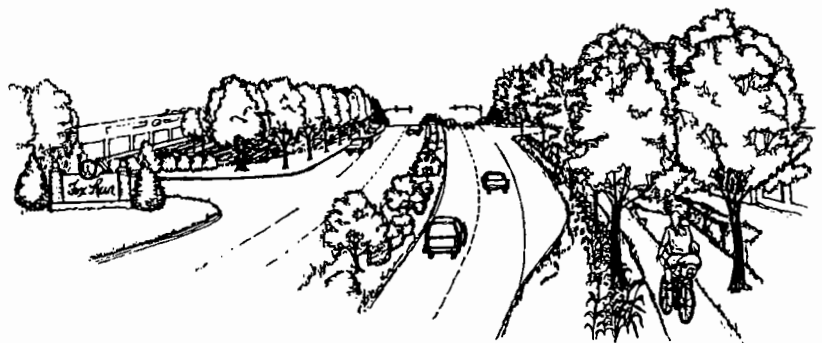
### 3. Special Design Standards for the Entry District

- a. In order to help facilitate the objectives of the adopted Transportation Plan and to maximize the amount of space available to businesses, the primary focus for new construction along the west side of Rte 2/4 shall be toward the proposed new road.

#### Exception:

Buildings on lots that front on Rte 2/4 may also be oriented toward the highway (dual frontage). However, they shall be designed to include provisions for access to the buildings from the secondary roadway as well.

- b. The number of entrances along both Rte 2/4 and the proposed new road shall be kept to a minimum by utilizing shared entrances and connected parking lots. The spacing between entrances along the proposed new road shall be no less than 400 feet. Spacing between entrances along Rte 2/4 is subject to State Highway Administration approval.



## F. VILLAGE DISTRICT

### 1. Existing Situation

The Village District has stores, offices, service establishments, restaurants, a movie theater, the Senior Citizens Center and a number of residences. Almost all are built at a "pedestrian scale". However, the area does not function as a village or a business center at the present time. It is almost exclusively automobile oriented and there are few if any places to walk or ride bicycles safely. There is no recognizable Village Center.

### 2. Future Development Guidelines

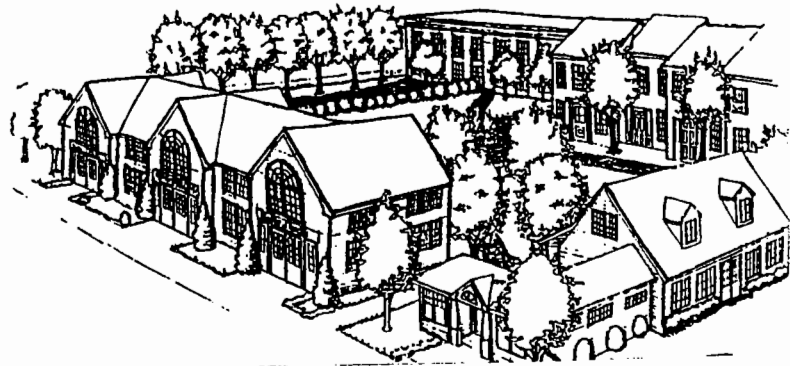
Major emphasis will be placed upon realizing the potential of this District to function as a pleasant, attractive village -similar to the Old Town District in terms of building orientation and scale but more contemporary in overall design.

New site designs will be expected to provide safe, convenient and attractive pedestrian and bicycle travel linkages as well as attractive outdoor activity areas. Mixed-use projects that combine retail, service, office and residential uses are strongly encouraged.

In accordance with the Prince Frederick Transportation Plan, a new road will be constructed west of Rt 2/4 which will eventually connect Stoakley Road to Dares Beach Road and Rt 231.

### 3. Special Development Standards for the Village District

- a. Buildings shall be oriented toward the proposed new road or perpendicular roads. Buildings on lots with frontage along Rt 2/4 may also be oriented toward the highway provided that access is provided to the proposed new road as well.
- b. The number of entrances along both Rt 2/4 and the proposed new road shall be kept to a minimum by utilizing shared entrances and connected parking lots. The spacing between entrances along the proposed new road shall be no less than 400 feet. Spacing between entrances along Rt 2/4 is subject to SHA approval.
- c. In mixed use projects, the applicant may, with approval of the Planning Commission, include public plazas, village greens and courtyards serving the general public as part of the recreational open space requirement for that project.
- d. Up to a 20% reduction in parking space requirements may be permitted provided that adequate parking is available within 400 feet of the proposed project.



## G. NEW TOWN DISTRICT

### 1. Existing Situation

The New Town District is largely undeveloped at the present time although Calvert High School, the Planetarium and the Calvert Auditorium are located here. A large tract within the New Town District is owned by the County. A segment of the old Baltimore & Drum Point railroad right-of-way is located within this district.

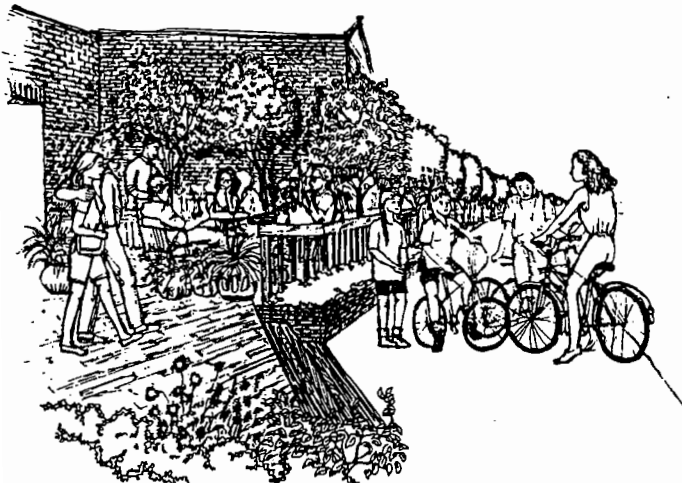
### 2. Future Development Guidelines

The New Town District offers a unique opportunity to create a comprehensively designed, mixed-use, planned community at a key location within the Town Center. In order to encourage innovative design, Zoning Ordinance and Subdivision regulations governing density, site design and permitted uses will be waived provided the entire District is comprehensively designed. The final design will be subject to Planning Commission and County Commissioner approval. Participation is voluntary.

A road running parallel to Rte 2/4 will be constructed through the New Town District. The Road will be designed as a limited access boulevard with landscaped median.

### 3. Special Development Standards for the New Town District

If the New Town District is not comprehensively designed as a single parcel, or if owners of individual parcels do not participate in a comprehensively designed project, the provisions of the Village District shall apply.



## H. FOREST DISTRICT

### 1. Existing Situation

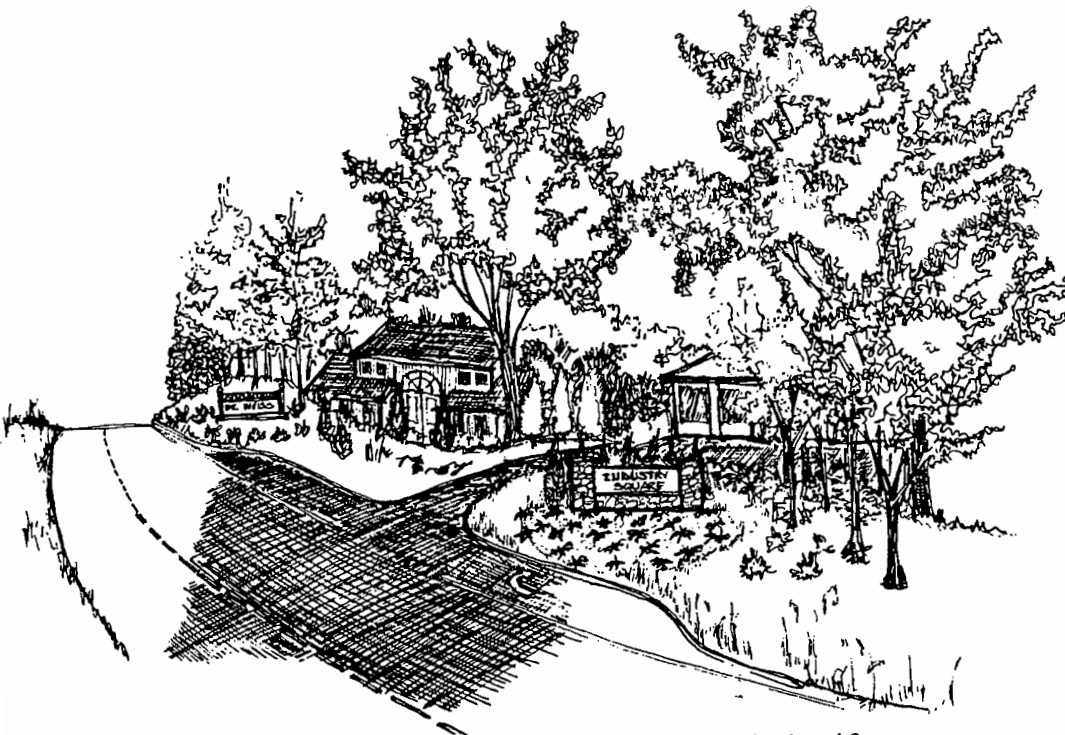
Most of the land within the Forest District is held in large undeveloped tracts. The existing forests within this district provide a soft, scenic edge along the Rte 2/4 corridor. A segment of the old Baltimore & Drum Point Railroad bed is located within this District.

### 2. Guidelines for Future Development

The Forest District is highly suitable as an employment center and can accommodate the same types of uses that are currently located within the Industrial Park. It is also highly suitable for residential developments and/or mixed-use developments. New developments that front on Rte 2/4 within this District will be expected to maintain the visual qualities of a forested edge.

### 3. Special Development Standards for the Forest District

Existing trees within the landscaping strip shall be retained. Selective thinning in order to provide visibility to highway oriented uses is permitted provided that a landscaping plan showing the extent of thinning is approved. In cases where there are no existing trees within the landscaping strip, a mix of hardwoods and evergreens designed to give the appearance of a natural landscape is required.



**KEY TO LAND USE CHARTS:**

	A blank indicates the use is not permitted	SC	Permitted use subject to special exception if it meets conditions
P	Permitted Use	S1	Permitted Use subject to special exception if less than 150 feet from a residential building or site where there is an active house permit or an established Historic District.
C	Permitted Use if it meets certain conditions	S2	Permitted Use subject to special exception if less than 300 feet from a residential building or site where there is an active house permit or an established Historic District.
S	Permitted Use subject to special exception from the Board of Appeals		

**PRINCE FREDERICK TOWN CENTER**

USE #	PRINCE FREDERICK TABLE OF LAND USES – AGRITOURISM, ECOTOURISM, AND HERITAGE TOURISM USES	OLD TOWN	OLD TOWN RESIDENTIAL	OLD TOWN TRANSITIONAL	FAIR-GROUND	ENTRY	VILLAGE	NEW TOWN	FOREST
1.	Agritourism Enterprise								
2.	Campground, Farm								
3.	Canoe or Kayak Launching Site, Commercial								
4.	Commercial Kitchen, Farm								
5.	Ecotourism Enterprise								
6.	Farm Support Business, Less than 5,000 square feet								
7.	Farm Support Business, More than 5,000 square feet								
8.	Heritage Trail Displays								
9.	Hunting Service								
10.	Public Events/Public Assemblies on Farmland								
11.	Rental Facilities on Farms								
12.	Sports Practice Fields on a Farm								

PRINCE FREDERICK TOWN CENTER									
USE #	PRINCE FREDERICK TABLE OF LAND USES - AGRICULTURAL	OLD TOWN	OLD TOWN RESIDENTIAL	OLD TOWN TRANSITIONAL	FAIR-GROUND	ENTRY	VILLAGE	NEW TOWN	FOREST
1.	Animal Husbandry								
2.	Aquaculture, Freshwater and Land-based								
3.	Aquaculture, Marine/Estuarine								
4.	Commercial Greenhouse, Retail	P		P	P	P	P	P	P
5.	Commercial Greenhouse, Wholesale	P		P	P	P	P	P	P
6.	Commercial Kennel, with indoor facilities only								
7.	Commercial Kennel, with outdoor facilities								
8.	Commercial or Non-Profit Stable or Horseback-Riding Club								
9.	Commercial Raising of Dangerous or Wild Animals								
10.	Commercial Raising of Fur-bearing Animals								
11.	Farm	P	P	P	P	P	P	P	P
12.	Farm Brewery								
13.	Farm Building	P	P	P	P	P	P	P	P
14.	Farm Distillery								
15.	Farm Stand	C		C	C	C	C	C	C
16.	Farm Winery								
17.	Field Crops	P	P	P	P	P	P	P	P
18.	Forest Product Processing								
19.	Garden Center or Farm Supply Store, Less than 25,000 s.f.	P		P		P	P	P	P
20.	Garden Center or Farm Supply Store, More than 25,000 and less than 75,000 s.f.					P	P		
21.	Livestock Auction and/or Sales Barn								
22.	Livestock Auction by a Non-Profit Organization or Farm Owner								
23.	Nursery, Retail	P	P	P	P	P	P	P	P
24.	Nursery, Wholesale	P	P	P	P	P	P	P	P
25.	Tree Farming	P	P	P	P	P	P	P	P
26.	Veterinary Hospital or Clinic, Livestock					P	P	P	P
27.	Veterinary Hospital or Clinic, Small Animals and Household Pets					P	P	P	P

**PRINCE FREDERICK TOWN CENTER**

<b>USE #</b>	<b>PRINCE FREDERICK TABLE OF LAND USES - RESIDENTIAL</b>	<b>OLD TOWN</b>	<b>OLD TOWN RESIDENTIAL</b>	<b>OLD TOWN TRANSITIONAL</b>	<b>FAIR-GROUND</b>	<b>ENTRY</b>	<b>VILLAGE</b>	<b>NEW TOWN</b>	<b>FOREST</b>
1.	Apartment, Accessory	C	C	C	C	C	C	C	C
2.	Apartment, Accessory for Resident Watchman/Caretaker								
3.	Apartment, Accessory over a Business								
4.	Assisted Living Facility	P	P	P	P	P	P	P	P
5.	Bed & Breakfast Facility with up to 2 Bedrooms in Use	C	C	C	C	C	C	C	C
6.	Bed & Breakfast Facility with 3 to 5 Bedrooms in Use	C	C	C	C	C	C	C	C
7.	Boarding House	C	C	C	C	C	C	C	C
8.	Dwelling, Attached (Duplex, Triplex, Fourplex and Townhouse)	P		P	C	C	C	C	C
9.	Dwelling, Attached: Multi-family			C	C	C	C	C	C
10.	Dwelling, Single Family Detached	C	C	C	C	C	C	C	C
11.	Group Home	P	P	P	P	P	P	P	P
12.	Liveaboards								
13.	Lodgers in Residence (no more than 3)	P	P	P	P	P	P	P	P
14.	Manufactured Home Community								C
15.	Manufactured Home for Resident Watchman/Caretaker								
16.	Manufactured Home on Individual Lot								
17.	Manufactured Home or Recreational Vehicle (Emergency)	C	C	C	C	C	C	C	C
18.	Manufactured Home Subdivision								C
19.	Manufactured Home, Farm			C	C	C	C	C	C
20.	Tenant House		C	C	C	C	C	C	C
21.	Tenant Houses, Additional (no more than 2 additional)			SC	SC	SC	SC	SC	SC

PRINCE FREDERICK TOWN CENTER									
USE #	PRINCE FREDERICK TABLE OF LAND USES – COMMERCIAL RETAIL	OLD TOWN	OLD TOWN RESIDENTIAL	OLD TOWN TRANSITIONAL	FAIR-GROUND	ENTRY	VILLAGE	NEW TOWN	FOREST
1.	Agricultural Machinery, Service and/or Supplies	P		P	S2	P	P	P	P
2.	Antiques Sales	P	P	P	P	P	P	P	P
3.	Art Gallery, Less than 25,000 s.f.	P		P		P	P	P	P
4.	Art Gallery, More than 25,000 and less than 75,000 s.f.					P	P		
5.	Artisan's and Crafters' Market	C		C	C	C	C	C	C
6.	Auction Building					S1	S1	S1	S1
7.	Boat Dealership					S2	S2	S2	S2
8.	Farmers' Market	C		C	C	C	C	C	C
9.	Flea Market by Non-profit Organization	P	P	P	P	P	P	P	P
10.	Home Improvement Center, Less Than 25,000 s.f.	P		P	S2	P	P	P	P
11.	Home Improvement Center, More Than 25,000 s.f. and Less Than 75,000 s.f.					P	P		
12.	Home Improvement Center, More Than 75,000 s.f. with a maximum size limit of 120,000 s.f.					C	C		
13.	Manufactured Home Dealer								
14.	Mobile Food Sales	C		C		C	C	C	C
15.	Retail Commercial Building with Drive-up Facility, Less than 25,000 s.f.	P		P		P	P	P	P
16.	Retail Commercial Building with Drive-up Facility, More than 25,000 and less than 75,000 s.f.					P	P		
17.	Retail Commercial Building with Drive-up Facility, More than 75,000 s.f. with a maximum size limit of 120,000 s.f.					C	C		
18.	Retail Commercial Building, Less than 25,000 s.f.	P		P		P	P	P	P
19.	Retail Commercial Building, More than 25,000 and less than 75,000 s.f.					P	P		
20.	Retail Commercial Building, More than 75,000 s.f. with a maximum size limit of 120,000 s.f.					C	C		
21.	Retail Commercial Sale or Display Area, Outdoor	C		C		C	C	C	C
22.	Watermen's Market	C		C	C	C	C	C	C

PRINCE FREDERICK TOWN CENTER									
USE #	PRINCE FREDERICK TABLE OF LAND USES – BUSINESS & PERSONAL SERVICES	OLD TOWN	OLD TOWN RESIDENTIAL	OLD TOWN TRANSITIONAL	FAIR-GROUND	ENTRY	VILLAGE	NEW TOWN	FOREST
1.	Boat Service and/or Repair	S				P	P		P
2.	Boat Storage, Commercial					S	S	S	S
3.	Boatel								
4.	Commercial Kitchen (not associated with an Eating Establishment)	P	S1	P	S1	P	P	P	P
5.	Commercial Pier								
6.	Commercial Trade or Business School	P	S1	P	S1	P	P	P	P
7.	Corporate Headquarters, less than 5,000 s.f.	P	S1	P	S1	P	P	P	P
8.	Corporate Headquarters, more than 5,000 s.f.	P		P		P	P	P	P
9.	Crematorium	P		P		P	P	P	P
10.	Eating Establishment with Drive-up Facility					P	P	P	P
11.	Eating Establishment without Drive-up Facility	P	S	P	S	P	P	P	P
12.	Entertainment Business, Adult								
13.	Flex Space Business	SC		SC	SC	C	C	C	
14.	Funeral Home	P		P		P	P	P	P
15.	Home Occupation, All Employed are Residents	C	C	C	C	C	C	C	C
16.	Home Occupation, 1 Equivalent Full-time non-resident employee	C	C	C	C	C	C	C	C
17.	Home Occupation, 2 Equivalent Full-time non-resident employees	C	SC	C	SC	C	C	C	C
18.	Laundry / Laundromat	P		P		P	P	P	P
19.	Laundry, Industrial								
20.	Motel or Hotel	P		P		P	P	P	P
21.	Office, Non-Medical, Medical or Clinic, less than 5,000 s.f.	P	S1	P	S1	P	P	P	P
22.	Office, Non-Medical, Medical or Clinic, more than 5,000 s.f.	P		P		P	P	P	P
23.	Office Support Services, including printing, copying, faxing, internetworking, etc., less than 5,000 s.f.	P	S1	P	S1	P	P	P	P
24.	Office Support Services, including printing, copying, faxing, internetworking, etc., more than 5,000 s.f.	P		P		P	P	P	P
25.	Personal Services, less than 5,000 s.f.	P	S1	P	S1	P	P	P	P
26.	Personal Services, more than 5,000 s.f.	P		P		P	P	P	P
27.	Tavern, Nightclub, Lounge, Dance Hall	S2		S2		S2	S2	S2	S2
28.	Tavern, Nightclub, Lounge, Dance Hall with Outdoor Facilities such as Bars								

PRINCE FREDERICK TOWN CENTER									
USE #	PRINCE FREDERICK TABLE OF LAND USES – RECREATION	OLD TOWN	OLD TOWN RESIDENTIAL	OLD TOWN TRANSITIONAL	FAIR-GROUND	ENTRY	VILLAGE	NEW TOWN	FOREST
1.	Campground and/or Recreational Vehicle Camp								
2.	Carnival, Fair or Circus – Temporary, Less than 5 Acres				SC	SC	SC	SC	SC
3.	Carnival, Fair or Circus – Temporary, More than 5 Acres				C	C	C	C	C
4.	Commercial or Non-Profit Meeting Hall, Banquet Hall	P		P		P	P	P	P
5.	Convention Center	P		P		P	P	P	P
6.	Drive-in Theatre					S2			
7.	Golf Course								
8.	Golf, Driving Range					S2			
9.	Golf, Miniature					S	S	S	S
10.	Indoor Commercial Amusements: Arcade, Pool Hall, Bingo Hall					S	S	S	S
11.	Indoor Commercial Amusements: Bowling, Skating Rink, Theatre	P		P	S1	P	P	P	P
12.	Indoor Commercial Amusements: Fitness Center	P		P	P	P	P	P	P
13.	Indoor Commercial Amusements: Studio, Commercial - Performing Arts	P	P	P	P	P	P	P	P
14.	Indoor Commercial Amusements: Swimming Pool, Athletic Courts	P		P	P	P	P	P	P
15.	Marina								
16.	Outdoor Recreation, such as: Swimming Pools, Athletic Courts					S1	P	P	P
17.	Retreat, Day	P		P		P	P	P	P
18.	Studio, Commercial	P	P	P	P	P	P	P	P
19.	Target Range, Indoor								
20.	Target Range, Outdoor								

**PRINCE FREDERICK TOWN CENTER**

USE #	PRINCE FREDERICK TABLE OF LAND USES – COMMERCIAL WHOLESALE	OLD TOWN	OLD TOWN RESIDENTIAL	OLD TOWN TRANSITIONAL	FAIR-GROUND	ENTRY	VILLAGE	NEW TOWN	FOREST
1.	Mini-Storage					P	P	P	P
2.	Warehouse, Indoor			P		P	P	P	P
3.	Warehouse, Outdoor					P	P	P	P
4.	Wholesale Lumber and/or Other Building Materials, Less Than 25,000 s.f.	P		P	S2	P	P	P	P
5.	Wholesale Lumber and/or other Building Materials, More Than 25,000 s.f. and Less Than 75,000 s.f.					P	P		
6.	Wholesale Lumber and/or other Building Materials, More Than 75,000 s.f. with a maximum size limit of 120,000 s.f.					C	C		
7.	Wholesaling, Indoor Only			P		P	P	P	P

**PRINCE FREDERICK TOWN CENTER**

USE #	PRINCE FREDERICK TABLE OF LAND USES - MOTOR VEHICLE & RELATED SERVICES	OLD TOWN	OLD TOWN RESIDENTIAL	OLD TOWN TRANSITIONAL	FAIR-GROUND	ENTRY	VILLAGE	NEW TOWN	FOREST
1.	Automobile Filling Station	SC		SC		C	C	C	C
2.	Automobile Filling Station with Convenience Store and/or Eating Establishment	SC		SC		C	C	C	C
3.	Automobile Parking Lot / Garage as a Principal Use	P	P	P	P	P	P	P	P
4.	Automobile Parts Dismantling and/or Storage								
5.	Automobile Repair/Service Shop without fuel sales	SC		SC		C	C	C	C
6.	Bus lot or garage					C	C	C	C
7.	Car Wash					C	C	C	C
8.	Commuter Parking Lot	P		P	P	P	P	P	P
9.	Inoperative Vehicle, 1 per lot	P	P	P	P	P	P	P	P
10.	Inoperative Vehicles, 2 per lot								
11.	Motor Vehicle Accessory Shop	C		C		C	C	C	C
12.	Motor Vehicle Dealership- New or Used	C		C		C	C	C	C
13.	Other Motor Vehicle Related Uses including Bus depot, taxi service, vehicle rental or leasing					C	C	C	C
14.	Park-and-Sell Lot	C		C		C	C	C	C
15.	Parking of Commercial Motor Vehicles	C	C	C	C	C	C	C	C
16.	Storage of Motor Vehicles					P			P
17.	Truck, Bus and Diesel Service and Repair Shop					P			P
18.	Truck Terminal								
19.	Vehicle Ferry Service								

**PRINCE FREDERICK TOWN CENTER**

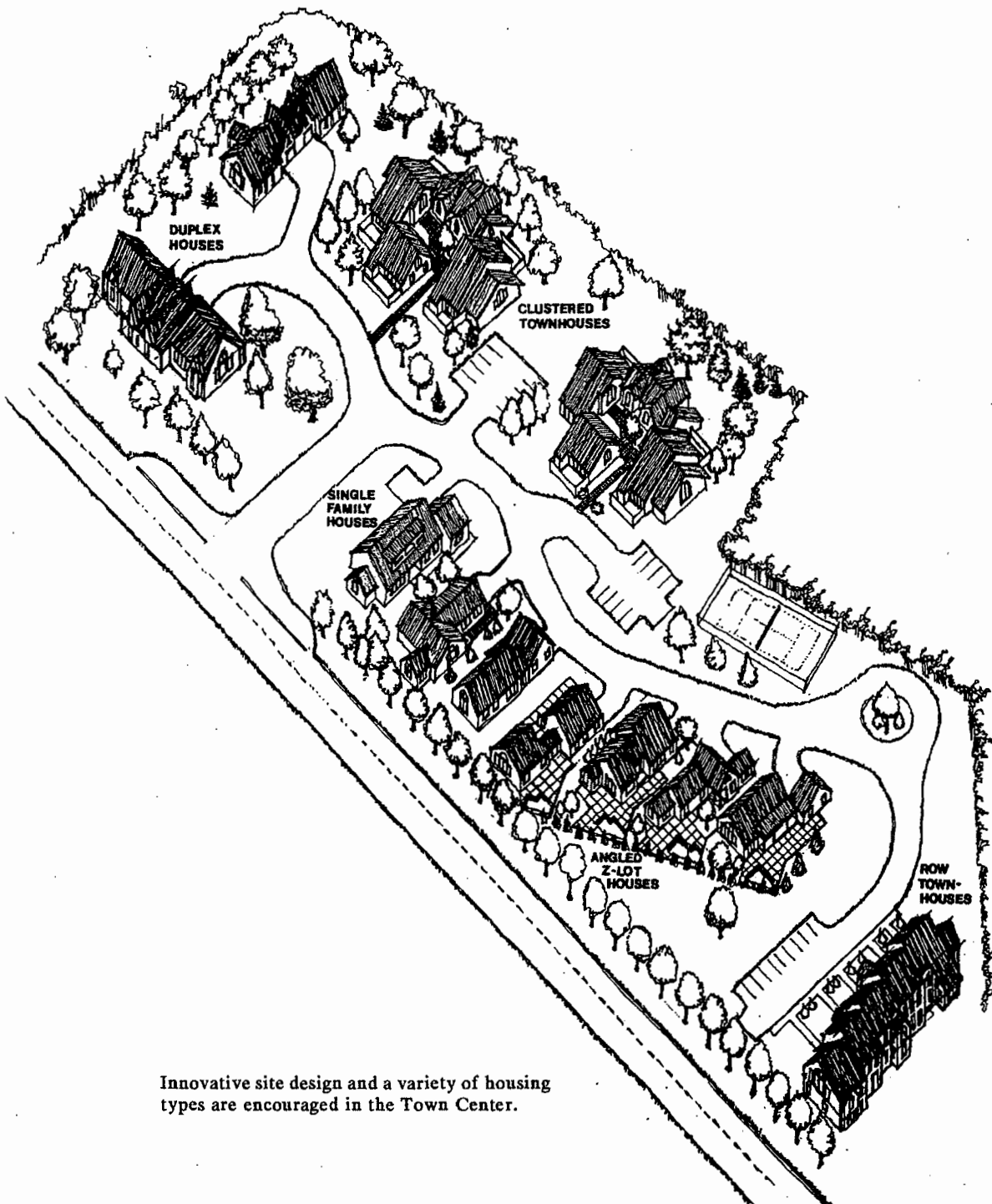
<b>USE #</b>	<b>PRINCE FREDERICK TABLE OF LAND USES - INDUSTRIAL</b>	<b>OLD TOWN</b>	<b>OLD TOWN RESIDENTIAL</b>	<b>OLD TOWN TRANSITIONAL</b>	<b>FAIR-GROUND</b>	<b>ENTRY</b>	<b>VILLAGE</b>	<b>NEW TOWN</b>	<b>FOREST</b>
1.	Agricultural/Seafood/Livestock Processing Plant								
2.	Asphalt Plant								
3.	Commercial Fuel Storage Business					S			
4.	Commercial Recycling Facility								
5.	Distillation of Alcohol as a Fuel, Commercial								
6.	Distillation of Alcohol as a Fuel On a Farm for Farm Use Only			C	C	C			
7.	Grain Elevator								
8.	Kiln, Wood Drying								
9.	Landfill, Land-Clearing Debris								
10.	Landfill, Rubble								
11.	Landfill, Sanitary								
12.	Manufacturing and/or Assembly, Heavy								
13.	Manufacturing and/or Assembly, Light, Less than 5,000 square feet	S1		S1	S2	C	C	C	
14.	Manufacturing and/or Assembly, Light, More than 5,000 square feet					C	C	C	
15.	Manufacturing and/or Assembly, Watercraft, Commercial					S2	S2	S2	
16.	Outdoor Storage in Connection with Commercial and/or Industrial Uses	P		P		P	P	P	P
17.	Power Generating Facility Accessory to a Residence or Business	C	C	C	C	C	C	C	C
18.	Power Generating Facility, Commercial								
19.	Research & Development Facility, Environmental	P	S1	P	S1	P	P	P	
20.	Research & Development Facility, Other	P	S1	P	S1	P	P	P	
21.	Salvage and/or Junk Yard								
22.	Sand, Gravel or Mineral Extraction and Processing								
23.	Sand, Gravel or Mineral Extraction (No Processing)					SC	SC	SC	
24.	Sawmill, Commercial	SC				P			
25.	Sawmill, Portable								
26.	Storage of Machinery & Equipment in Connection With Excavating and /or Contracting Business					S			S

**PRINCE FREDERICK TOWN CENTER**

<b>USE #</b>	<b>PRINCE FREDERICK TABLE OF LAND USES – INSTITUTIONAL</b>	<b>OLD TOWN</b>	<b>OLD TOWN RESIDENTIAL</b>	<b>OLD TOWN TRANSITIONAL</b>	<b>FAIR-GROUND</b>	<b>ENTRY</b>	<b>VILLAGE</b>	<b>NEW TOWN</b>	<b>FOREST</b>
1.	College or University	P	P	P	P	P	P	P	P
2.	Day Care Center:								
3.	12 Clients or Less	P	P	P	P	P	P	P	P
4.	13 to 19 Clients	P	S	P	P	P	P	P	P
5.	20 or More Clients				S	P	P	P	P
6.	Elementary or Secondary School	P	P	P	P	P	P	P	P
7.	Fire and/or Rescue Service	P		P		P	P	P	P
8.	Hospital	P		P		P	P	P	P
9.	Library	P	P	P	P	P	P	P	P
10.	Museum	P	P	P	P	P	P	P	P
11.	Nursing or Convalescent Home	P	P	P	P	P	P	P	P
12.	Place of Worship, Parish Hall, Convent, Monastery or Rectory	P	P	P	P	P	P	P	P
13.	Public or Governmental Building	P	P	P	P	P	P	P	P
14.	Public or Non-profit Park and/or Recreation Area Including: Tennis Courts, Swimming Pools, Athletic Fields, etc.	P	P	P	P	P	P	P	P
15.	Public Utility Lines & Accessory Structures	S2	S2	S2	S2	S2	S2	S2	S2
16.	Solid Waste Collection Site					C	C	C	C
17.	Temporary Structure Incidental to Schools (classroom relocatables)	P	P	P	P	P	P	P	P
18.	Wastewater Treatment Facility								
19.	Water Supply Treatment Facility	P	P	P	P	P	P	P	P

**PRINCE FREDERICK TOWN CENTER**

<b>USE #</b>	<b>PRINCE FREDERICK TABLE OF LAND USES – UNCLASSIFIED</b>	<b>OLD TOWN</b>	<b>OLD TOWN RESIDENTIAL</b>	<b>OLD TOWN TRANSITIONAL</b>	<b>FAIR-GROUND</b>	<b>ENTRY</b>	<b>VILLAGE</b>	<b>NEW TOWN</b>	<b>FOREST</b>
1.	Accessory Building or Use	P	P	P	P	P	P	P	P
2.	Airport or Landing Field								
3.	Cemetery or Memorial Garden	C		C		P			P
4.	Communications Towers & Antennas (Government, Commercial & Private)	SEE SECTION 3-3 OF THE CALVERT COUNTY ZONING ORDINANCE							
5.	Dock, Pier, Private								
6.	Flea Market by Non-Profit Organization	P	P	P	P	P	P	P	P
7.	Garage Sale, Yard Sale or Estate Sale	C	C	C	C	C	C	C	C
8.	Heliport	S	S	S	S	S	S	S	S
9.	Household Pets	P	P	P	P	P	P	P	P
10.	Livestock Kept as Pets								
11.	Model Home	C	C	C	C	C	C	C	C
12.	Structure for the Keeping of Animals	P	P	P	P	P	P	P	P
13.	Temporary Recreational Vehicle at Construction site (for watchman)	C	C	C	C	C	C	C	C
14.	Temporary Structure Incidental to Construction (non-residential)	C	C	C	C	C	C	C	C
15.	Temporary Structure Incidental to Sales or Rentals in New Residential or Commercial Developments (sales trailer)								
16.	Unoccupied Recreational Vehicle	C	C	C	C	C	C	C	C
17.	Wild or Dangerous Animals Kept as Pets								



Innovative site design and a variety of housing types are encouraged in the Town Center.

## CONDITIONAL USES

The following are conditions imposed upon land uses indicated in the Table of Land Uses. If a use is listed as conditional in the Land Use Tables (indicated by a 'C' or an 'SC'), and no conditions are listed below, the conditions listed in Section 3-1 of the Calvert County Zoning Ordinance (CCZO) apply. The conditions listed below are unique to the Prince Frederick Town Center. Note: This section of the Prince Frederick Town Center Zoning Ordinance has been renumbered for consistency with the Calvert County Zoning Ordinance.

Agricultural Use #15     Farm Stand – See CCZO for conditions.

Residential Use #1     Apartment, Accessory – See CCZO for conditions.

Residential Use #5     Bed & Breakfast Facility with up to 2 Bedrooms in Use – See CCZO for conditions.

Residential Use #6     Bed & Breakfast Facility with 3 to 5 Bedrooms in Use – See CCZO for conditions.

Residential Use #7     Boarding House, provided that the following conditions are met:

1. An owner lives on the premises,
2. The facility is a part of the dwelling unit with the exception that existing non-conforming guest houses may be used,
3. No separate kitchens are provided,
4. Adequate off-street parking is provided,
5. Fire Marshall and Health Department approvals are obtained, and
6. An Occupancy Permit for such use is obtained.

Residential Use #8     Dwelling, Attached (Duplex, Triplex, Fourplex and Townhouse):

1. In Fairgrounds District, provided that at least 40 percent of the units on the site are single family detached dwellings.
2. In Entry District and Village District and New Town District provided that at least 50 percent of the site is reserved for commercial, office or industrial use.
3. In Forest District provided that dwelling units are part of a comprehensively designed community where 30 percent of the dwelling units are single family detached, 30 percent are single-family attached and 30 percent are multifamily. The remaining 10 percent may be any of the above. The Planning Commission may waive this requirement if it can be demonstrated that the proposed development will contribute to a housing mix within the Town Center as a whole that is roughly equal to the required housing mix on the site.

Residential Use #9     Dwelling, Attached: Multi-Family:

1. In Entry District, Village District and New Town District provided that at least 50 percent of the site is reserved for commercial, office or industrial use.
2. In Old Town Transition District provided that:
  - a. at least 50 percent of the site is reserved for commercial or office use (see footnote 1), and
  - b. the site is immediately adjacent to Route 2/4 or Dares Beach Road.
3. In Fairgrounds District provided that no more than 20 percent of the dwelling units on the site are multi-family dwellings.
4. In Forest District, provided that dwelling units are part of a comprehensively designed community where 30 percent of the dwelling units are single family detached, 30 percent are single-family attached and 30 percent are multifamily. The remaining 10 percent may be any of the above. The Planning Commission may waive this requirement if it can be demonstrated that the proposed development will contribute to a housing mix within the Town Center as a whole that is roughly equal to the required housing mix on the site.

- Residential Use #10 Dwelling, Single-family Detached:
1. In Old Town District, Old Town Residential District, Fairgrounds District, and Old Town Transition District provided that at least 50 percent of the length of the building is 20 feet wide, excluding porches.
  2. In Entry District, Village District and New Town District provided that:
    - a. At least 50 percent of the site is reserved for commercial, office or industrial use<sup>1</sup> and
    - b. At least 50 percent of the length of the building is 20 feet wide, excluding porches.
  3. In Forest District provided that:
    - a. dwelling units are part of a comprehensively designed community where 30 percent of the dwelling units are single family detached, 30 percent are single-family attached and 30 percent are multifamily. The remaining 10 percent may be any of the above. The Planning Commission may waive this requirement if it can be demonstrated that the proposed development will contribute to a housing mix within the Town Center as a whole that is roughly equal to the required housing mix on the site, and
    - b. at least 50 percent of the length of the building is 20 feet wide, excluding porches.
- Residential Use #14 Manufactured Home Community, (conditional in the Forest District) provided that it meets the Regulations in the Manufactured Home Rental Communities Ordinance. (Section 5-7 of the Calvert County Zoning Ordinance).
- Residential Use #17 Manufactured Home or Recreational Vehicle (Emergency) – See CCZO for conditions.
- Residential Use #18 Manufactured Home Subdivision – See CCZO for conditions.
- Residential Use #19 Manufactured Home, Farm – See CCZO for conditions.
- Residential Use #20 Tenant House – See CCZO for conditions.
- Residential Use #21 Tenant Houses, Additional (no more than 2 additional) – See CCZO for conditions.
- Commercial Retail Use #5 Artisan's and Crafters' Market – See CCZO for conditions.
- Commercial Retail Use #8 Farmers' Market – See CCZO for conditions.
- Commercial Retail Use #12 Home Improvement Center, more than 75,000 square feet with a maximum size of 120,000 square feet, provided that the conditions for "Retail Commercial Buildings, more than 75,000 square feet with a maximum size of 120,000 square feet" are met.
- Commercial Retail Use #14 Mobile Food Sales – See CCZO for conditions.
- Commercial Retail Use #17 Retail Commercial Building with Drive-up Facility, more than 75,000 s.f. with a maximum size limit of 120,000 s.f., provided that it meets the conditions for "Retail Commercial Buildings, More than 75,000 s.f. with a maximum size limit of 120,000 s.f."
- Commercial Retail Use #20 Retail Commercial Buildings, more than 75,000 square feet with a maximum size of 120,000 square feet, provided that the following conditions are met:
1. Building and Site Design
 

In addition to specific requirements of the Town Center Zoning Ordinance and Article 6 of the County Zoning Ordinance, the following provisions shall apply:

    - a. Any outdoor sales areas shall be specifically designed as such and shall include fencing and landscaping around their entire perimeter in accordance with the Architectural Review Standards of this Town Center Zoning Ordinance. They shall not impede pedestrian circulation and shall not be located within parking lots.

<sup>1</sup> The requirement that at least 50 percent of the site be reserved for commercial office or industrial use may be waived if the Planning Commission determines that the site is not suitable for commercial use as measured in terms of generally recognized marketing standards. These standards include visibility, accessibility, potential site development costs, and neighboring uses.

- b. An outdoor area or areas totaling a minimum of 2000 sq. ft. shall be provided for a bus stop and outdoor seating. The area(s) shall be landscaped and shall be conveniently located near the public entrance and pedestrian travelways.
- c. All sides having a public entrance shall include some combination of the following features along no less than 70 percent of the horizontal length of any façade with a public entrance: recesses, projections, awnings, arcades, display windows and entry areas. (see Guidelines Appendix attached to this Town Center Ordinance).

2. Maintenance Agreement

Before a building permit is issued, a maintenance agreement shall be executed between the landowner and the Board of County Commissioners, which agreement shall apply whether the building is fully occupied, partially occupied or vacant. The agreement shall provide for the maintenance of the building exterior, signage, lighting, landscaping, parking lots, sidewalks, and trash removal. It shall contain provisions whereby the County shall give the landowner notice of any violations of the agreement and a reasonable time to correct the violation. The agreement shall further provide that if the violation has not been corrected within the specified time, the County shall be permitted to enter the premises, correct the violation and add the costs incurred to the landowner's next tax bill. The agreement shall be recorded among the land records of Calvert County and shall be binding on all subsequent owners so long as the building creating the need for the agreement remains standing.

3. Co-location<sup>2</sup>

For the purposes of these regulations, co-location is defined as: The placement of smaller retail and service buildings with individual outside entrances on the same site as a large (75,000 sq. ft. +) retail building. The total square footage of the smaller buildings shall equal at least 20 percent of the large retail building. (See design guidelines for examples of co-location.)

The square footage of the co-located buildings shall be excluded from the maximum square footage permitted by the Land Use Charts.

a. Waivers for Co-location

The Planning Commission may waive the requirements for co-location in cases where a large retail store in existence as of August 10, 2004, is to be expanded on-site by no more than 50% of its gross square footage. No waivers shall be granted for (1) the expansion of a large retail store in existence as of August 10, 2004, if the expansion is more than 50% of its gross square footage or (2) the expansion of large retail stores constructed after August 10, 2004. Such waivers shall only be granted if the applicant demonstrates that an undue hardship will be imposed due to exceptional narrowness, shallowness, or shape of particular parcels of property or by reason of exceptional topographical conditions or other extraordinary situations or conditions affecting the property. Self-imposed hardship and financial hardship shall not be considered.

Commercial Retail Use #21 Retail Commercial Sale or Display Area, Outdoor – See CCZO for conditions.

Commercial Retail Use #22 Watermen's Market – See CCZO for conditions.

Business & Personal Services Use #13 Flex Space Business, provided that:

- 1. in the Old Town, Old Town Transitional, and Fairgrounds District, the size of the building shall be limited to 5,000 square feet; and
- 2. in all Districts, the conditions of the Calvert County Zoning Ordinance for Flex Space Businesses shall be met; and
- 3. noise, odor and glare shall be comparable to or less than retail commercial uses of similar size.

<sup>2</sup> The requirements for co-location may apply to each phase of a development or to the entire site, at the developer's discretion.

Business & Personal Services Use #15	<u>Home Occupation, All Employed are Residents</u> – See CCZO for conditions.
Business & Personal Services Use #16	<u>Home Occupation, 1 Equivalent Full-time non-resident employee</u> – See CCZO for conditions.
Business & Personal Services Use #17	<u>Home Occupation, 2 Equivalent Full-time non-resident employees</u> – See CCZO for conditions.
Recreation Use #2	<u>Carnival, Fair or Circus – Temporary, Less than 5 acres</u> – See CCZO for conditions.
Recreation Use #3	<u>Carnival, Fair or Circus – Temporary, Mess than 5 acres</u> – See CCZO for conditions.
Commercial Wholesale Use #6	<u>Wholesale Lumber and/or other Building Materials, More than 75,000 s.f. with a maximum size limit of 120 square feet</u> , provided that the conditions for “Retail Commercial Buildings, more than 75,000 square feet with a maximum size of 120,000 square feet” are met.
Motor Vehicle Use #1	<u>Automobile Filling Station</u> , provided that the following conditions are met: <ol style="list-style-type: none"> <li>1. No fuel pump, oil draining pit, or other visible appliance for servicing automobiles is located within 25 feet of the front lot line,</li> <li>2. No structure or building is erected within 80 feet of any dwelling. Neither setback may be reduced,</li> <li>3. No new service bay openings shall face a right-of-way,</li> <li>4. No more than ten inoperative vehicles and/or junk cars are allowed, except those which are 100 percent screened from adjoining properties and rights-of-way,</li> <li>5. Junk vehicles shall be removed after 30 days, and</li> </ol>
Motor Vehicle Use #2	<u>Automobile Filling Station with Convenience Store and/or Eating Establishment</u> , provided that the conditions for “Automobile Filling Station” are met.
Motor Vehicle Use #5	<u>Automobile Repair Shop/Service Shop without fuel sales</u> , provided that the following conditions are met: <ol style="list-style-type: none"> <li>1. No oil draining pit, or other visible appliance for servicing automobiles is located within 25 feet of the front lot line,</li> <li>2. No structure or building is erected within 80 feet of any dwelling. Neither setback may be reduced,</li> <li>3. No new service bay openings shall face a right-of-way,</li> <li>4. No more than ten inoperative vehicles and/or junk cars are allowed, except those which are 100 percent screened from adjoining properties and rights-of-way,</li> <li>5. Junk vehicles shall be removed after 30 days, and</li> </ol>
Motor Vehicle Use #6	<u>Bus lot or garage</u> – See CCZO for conditions.
Motor Vehicle Use #7	<u>Car Wash</u> , provided that parking areas for commercial or customer vehicles shall either meet the landscaping requirements for parking lots or the landscaping requirements for outdoor storage areas.
Motor Vehicle Use #11	<u>Motor Vehicle Accessory Shop</u> – See CCZO for conditions.
Motor Vehicle Use #12	<u>Motor Vehicle Dealer - New or Used</u> , provided that parking areas for automobiles that are for sale shall <u>either</u> meet the landscaping requirements for parking lots <u>or</u> the landscaping requirements for outdoor storage areas. Notwithstanding the above, a landscaped display area capable of displaying up to five vehicles may be located adjacent to the right-of-way.
Motor Vehicle Use #13	<u>Other Motor Vehicle Related Uses including bus depot, taxi service, vehicle rental or leasing</u> , provided that parking areas for commercial or customer vehicles shall either meet the landscaping requirements for parking lots or the landscaping requirements for outdoor storage areas.

Motor Vehicle Use #14	<u>Park-and-Sell Lot</u> , provided that parking areas for automobiles that are for sale shall <u>either</u> meet the landscaping requirements for parking lots <u>or</u> the landscaping requirements for outdoor storage areas. Notwithstanding the above, a landscaped display area capable of displaying up to five vehicles may be located adjacent to the right-of-way.
Motor Vehicle Use #15	<u>Parking of Commercial Motor Vehicles</u> – See CCZO for conditions.
Industrial Use #6	<u>Distillation of Alcohol as a Fuel On a Farm for Farm Use Only</u> – See CCZO for conditions.
Industrial Use #13	<u>Manufacturing and/or Assembly, Light, less than 5,000 square feet</u> , provided that potential noise, odor and glare are comparable to retail commercial uses of similar size.
Industrial Use #14	<u>Manufacturing and/or Assembly, Light, more than 5,000 square feet</u> , provided that potential noise, odor and glare are comparable to retail commercial uses of similar size.
Industrial Use #17	<u>Power Generating Facility Accessory to a Residence or Business</u> – See CCZO for conditions.
Industrial Use #23	<u>Sand, Gravel or Mineral Extraction (No Processing)</u> – See CCZO for conditions.
Industrial Use #24	<u>Sawmill, Commercial</u> , provided that the following conditions are met: <ol style="list-style-type: none"> <li>1. Such use is not nearer than 500 feet from any property boundary; and</li> <li>2. Such use is not nearer than 100 feet from any road maintained by the County or State; and</li> <li>3. Adequate measures are taken for the abatement of obnoxious or offensive odor, dust, smoke, noise, vibration, or similar nuisance, and protection against fire is employed.</li> </ol>
Institutional Use #16	<u>Solid Waste Collection Sites</u> , provided that they are in accordance with the Comprehensive Solid Waste Management Plan and do not exceed 100 square feet in size.
Unclassified Use #3	<u>Cemetery or Memorial Garden</u> (conditional in Old Town District, Old Town Transitional District, Old Town Residential District, and Fairgrounds District), provided that it is an expansion of an existing cemetery or memorial garden.
Unclassified Use #7	<u>Garage Sale, Yard Sale or Estate Sale</u> – See CCZO for conditions.
Unclassified Use #11	<u>Model Home</u> – See CCZO for conditions.
Unclassified Use #13	<u>Temporary Recreational Vehicle at Construction Site (for watchman)</u> – See CCZO for conditions.
Unclassified Use #14	<u>Temporary Structure Incidental to Construction (non-residential)</u> – See CCZO for conditions.
Unclassified Use #16	<u>Unoccupied Recreational Vehicle</u> – See CCZO for conditions.

## VIII. ARCHITECTURAL REVIEW PROCEDURES

### A. ARCHITECTURAL STANDARDS ESTABLISHED

During the public meetings held as preparation for the Prince Frederick Master Plan, one of the views most frequently expressed by the residents and business people of Prince Frederick was the desire for architectural standards.

Architectural Standards are based on the premise that the overall appearance of a community bears a direct relationship to the social well being of its residents, affects property values and provides economic opportunities. In view of its impact on the community, control over the appearance of a community should not be limited to the few who are directly involved in building construction, but should be vested in the general citizenry through publicly mandated, legally adopted architectural standards.

### B. AUTHORIZATION FOR ARCHITECTURAL REVIEW

In accordance with the provisions in Article 66B, Annotated Code of Maryland, the Board of County Commissioners has the authority under Article 4.01 (b) "upon the zoning or rezoning of any land or lands pursuant to the provisions of this article, (to) impose such additional restrictions, conditions, or limitation as may be deemed appropriate to preserve, improve or protect the general character and design of the lands and improvements being zoned or rezoned...and may, upon the zoning or rezoning of any land or lands, retain or reserve the power and authority to approve or disapprove the design of buildings, construction, landscaping, or other improvements, alterations, and changes made or to be made on the subject land or

lands to assure conformity with the intent and purpose of this article and of the jurisdiction's zoning ordinance. The powers provided in subsection (b) of this section shall be applicable only if the local legislative body adopts an ordinance which shall include enforcement procedures and requirements for adequate notice of public hearings and conditions sought to be imposed.

Section 7.01 (a) of Article 66B enables the local legislative body to "provide by ordinance for the enforcement of this article (66B) and of any ordinance or regulation made thereunder."

### C. APPOINTMENT OF ARCHITECTURAL REVIEW COMMITTEE

The Board of County Commissioners shall appoint an Architectural Review Committee whose functions shall be to:

1. make recommendations to the Planning Commission regarding approval of exterior design features of buildings, structures, and signs on the basis of the appearance standards and criteria;
2. promote awareness of good design within the community;
3. advise the County on matters relating to the design of public amenities within Town Centers and any other matters relating to appearance within Town Centers; and
4. review Master Plans and make recommendations.

D. COMPOSITION OF ARCHITECTURAL REVIEW COMMITTEE

The Architectural Review Committee shall be composed of five members. At least one member shall have experience in architecture, landscape architecture, historic preservation, urban design, planning, engineering or related fields. No more than two members may be from areas more than five miles away from the Town Center. At least one member shall be an owner of a business located within the Town Center and at least one member shall be a resident (within 5 miles) who is not connected with a business within the Town Center. Priority may be given to those who have participated in the Master Planning process. Exceptions in the criteria listed above may be made by the Board of County Commissioners in the event there are not enough qualifying applicants.

1. Tenure

The initial committee appointed by the County Commissioners shall serve for two-year, three-year and four-year terms and may be reappointed by the County Commissioners. Thereafter, members shall serve for four years and may be reappointed by the County Commissioners. The composition of the Board shall remain as above.

(3/12/96) 2. Officers

The Committee shall elect a Chairman and Vice-Chairman bi-annually. The Chairman and Vice-Chairman may be a Committee member or staff may be elected Chairman-Pro-Tem without voting authority. The Chairman-Pro-Tem may assign the Chairmanship as needed in his/her absence.

3. Meetings

The committee shall hold monthly meetings unless otherwise determined by a majority vote of the committee membership.

4. Jurisdiction (3/12/96)

The Architectural Review Committee shall have jurisdiction as described below. The Committee shall have authority to review existing rules and regulations governing landscaping and site design and make recommendations to the Planning Commission.

E. PROJECTS REQUIRING ARCHITECTURAL REVIEW (3/19/96)

The following projects require architectural review:

1. All new commercial, office, industrial, institutional and residential buildings and components of buildings such as windows, doors, decorative elements, and canopies;
2. Additions to existing commercial, office, industrial and institutional buildings;
3. Remodeling of existing commercial, office, industrial, and institutional buildings;
4. New fences and changes to existing fences visible from existing or proposed public roads;
5. New signs and changes to existing signs;
6. Additions to existing residential buildings visible from existing or proposed public roads and remodeling of existing residential buildings visible from public roads if they are located within the Old Town District, Old Town Residential District, Old Town Transition District or Fairgrounds District.

(03/12/96) The Committee shall also review proposed plans for public amenities such as public squares, landscaping plans, signs, and street furniture and shall make recommendations to the Planning Commission and Board of County Commissioners.

Exception:

Additions to existing structures which do not conform to the architectural standards listed in Chapter IV shall be permitted to be constructed in conformance with their own style provided that the addition is no more than 50% of the size of the original building.

(05/01/06) F. REVIEW PROCESS FOR PROJECTS REQUIRING ARCHITECTURAL REVIEW

1. Submittal of applications

Applicants for architectural review shall submit an application form together with required illustrations to the Department of Planning and Zoning a minimum of five days prior to the Architectural Review Committee meeting. Forms and list of specific requirements are available at the Department of Planning and Zoning.

Applications may be submitted at any time during the development process. However, applicants are strongly encouraged to submit their plans as early as possible in order to avoid any delays.

2. Review Procedure

The Prince Frederick Architectural Review Committee shall review applicable projects in accordance with the provisions of this Ordinance and make recommendations in writing to the Planning Commission. The Planning Commission shall make the final decision.

The Planning Commission may designate authority to its Chairperson or its Secretary to approve design features that are both consistent with the Master Plan and its Zoning Ordinance

and comply with the recommendations of the Architectural Review Committee.

Applicants or their representatives are required to attend and participate in discussions concerning their proposed projects. Projects will not be reviewed unless the applicant or their representative is present at the review meeting except upon request of the applicant.

Architectural review shall be limited to the external features of applicable buildings, structures other than buildings, and signs as listed in Section E. above. Architectural Review shall not include site plans or landscaping plans unless the committee is asked to make a recommendation by a reviewing agency or the Planning Commission.

3. Appeals

Any appeal from a decision of the Planning Commission for architectural review of a project associated with a Category I Site Plan (See Section 4-2.01.B.3 of the Calvert County Zoning Ordinance) shall be noted in the Circuit Court for Calvert County. Such appeal shall be taken according to the Maryland Rules as set forth in Title 7, Chapter 200, as amended from time to time, within 30 days of the Planning Commission's decision. Any appeal from a decision of the Planning Commission for architectural review of any project other than those associated with a Category I Site Plan may be appealed to the Board of Appeals. Such appeal shall be made in accordance with Section 11-1.04 of the Calvert County Zoning Ordinance within 30 days of the Planning Commission's decision.

4. Enforcement

Building permits may not be issued until Architectural approval is obtained. Failure to obtain a building permit for a use requiring permits and/or failure to comply with the requirements for architectural design is subject to the provisions in Section 1-7 of the Calvert County Zoning Ordinance.

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**A DESIGN CHECKLIST**



The following checklist is based upon the design elements presented in this ordinance. As you begin to design your development project, consider the following questions. You will be asked to address these questions during the review process.

\_\_\_\_\_

A. Does the project protect significant natural features? Does it incorporate existing topography and natural features into the subdivision or site design? Has grading and clearing been held to a minimum?

\_\_\_\_\_

B. Does the project provide safe, convenient and attractive vehicular and pedestrian circulation on the site? Does it provide vehicular and pedestrian access to adjacent sites and nearby activity areas? Does it clearly identify and define entrances and exits?

\_\_\_\_\_

C. How does the project acknowledge the unique characteristics of the District within which it is located?

\_\_\_\_\_

D. How does the project relate to other buildings in the neighborhood or vicinity? Are buildings oriented on the site in such a way as to establish a clearly understandable and attractive relationship between adjacent buildings and between buildings and the street? Is the building design compatible in terms of height, proportion, rhythm and scale? Will it be a good neighbor? Does it respect the privacy of existing neighbors? Does it help ensure privacy for residents within the proposed community?

\_\_\_\_\_

E. Does the project acknowledge the historic heritage and unique position of Prince Frederick as the County Seat through its architecture and site design?

\_\_\_\_\_